



# LONEDELL R-14 SCHOOL DISTRICT

Success; Nothing Less!

Chelsea Davis, Board President  
Amanda Bailey, Member  
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Brad Rucker, Vice-President  
Cody Causey, Member  
Dr. Monica Hiatt, Superintendent

James Heideman, Treasurer  
Andrea Dierker, Member  
Dr. Shannon Banderman, Principal

## June 4, 2026, Bus Garage Committee Meeting Minutes

### 1. Call to Order

Superintendent, Monica Hiatt, called the meeting to order at 9:06 a.m. in the board room.

### 2. Attendance

Stephanie York, Secretary to the Board, took attendance.

Committee Members Present:	Committee Members Absent:
Chelsea Davis, Board President Dr. Monica Hiatt, Superintendent Dennis Barry, Maintenance Coordinator	James Heideman, Board Treasurer Brad Rucker, Board Vice President

### Guests in Attendance:

Mark Ruether, Hoener Architects  
Melissa Wucher, Hoener Architects  
Ryan Schweissguth, Cochran Engineering  
Carter Schemal, ECS Consulting Engineers  
Dennis Harris, ECS Consulting Engineers  
Jason Bolzenius, ECS Consulting Engineers

### 3. Bus Garage Construction Document Discussion

The committee and guests had a 95% page turn review of the construction bid documents.

See Hoener's notes for details discussed.

### 4. Adjournment

The meeting ended at 12:15 p.m.

Date Approved June 22, 2024

Board President Chelsea Davis

Board Treasurer James E. Heideman

Submitted by Stephanie E. York

## MEETING NOTES

Project Name: **Lonedell Bus Garage** Meeting Date: **06/04/2026**

Project Location: **Londell, MO**

Project Number: **25-43B**

Attendees:	<u>Company</u>	<u>Name</u>	<u>Title</u>	<u>Email</u>
Distribution:	Lonedell R-XIV School District	Dr. Monica Hiatt	District Superintendent	<a href="mailto:mhiatt@lonedell.org">mhiatt@lonedell.org</a>
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		Dennis Barry	Maintenance Coordinator	<a href="mailto:dbarry@lonedell.org">dbarry@lonedell.org</a>
		Stephanie York	Business Manager	<a href="mailto:svork@lonedell.org">svork@lonedell.org</a>
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	ASDG	Nathan Kreke	Structural Engineer (virtual)	<a href="mailto:nkreke@asdgllc.com">nkreke@asdgllc.com</a>
	Hoener Architects	Mark Reuther	Architect of Record, President	<a href="mailto:mreuther@hoenerarchitects.com">mreuther@hoenerarchitects.com</a>
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	Carter Schemal	Mechanical [Plumbing] Engineer, Principal	<a href="mailto:carter@ecsstl.com">carter@ecsstl.com</a>	
	Lonedell R-XIV School District	Brad Rucker	Board Vice President	<a href="mailto:bradrucker@lonedell.org">bradrucker@lonedell.org</a>
		James Heideman	Board Treasurer	<a href="mailto:jheideman@lonedell.org">jheideman@lonedell.org</a>
	Hoener Architects	Bret Fox	Senior Designer	<a href="mailto:bfox@hoenerarchitects.com">bfox@hoenerarchitects.com</a>
		Jennifer Dopuch	Administrative Assistant	<a href="mailto:jdopuch@hoenerarchitects.com">jdopuch@hoenerarchitects.com</a>

### 95% PAGE TURN REVIEW MEETING

#### I. Purpose of Meeting

- a. Review the 95% Construction Document progress set with the District and consultant team, confirm design decisions, identify remaining coordination items, and establish action items required prior to bidding.

#### II. Specifications and Drawing Coordination

- a. The District identified several references to details and specifications that were not included in the review package. The design team explained that the architectural and structural specifications had not yet been fully incorporated into the progress review set.
  - i. Architectural and structural specifications will be incorporated into the final bid package.
  - ii. Design team to complete specification coordination and clean up drawing references.

#### III. Civil

- a. **Earthwork, Rock Excavation & Finish Floor Elevation Evaluation** - A significant portion of the meeting was devoted to reviewing the grading conditions at the south end of the bus port, the proposed finish floor elevation, anticipated import fill quantities, and potential rock excavation impacts. The team discussed the balance between maintaining the current building elevation, reducing imported fill, avoiding excessive rock excavation, and developing appropriate allowances for bidding.
  - i. Cochran Engineering noted that the current grading plan increased the anticipated imported fill quantity from the preliminary estimate to approximately 8,400 cubic yards, largely due to the developed grading plan and the additional parking / drive area extending farther east into the slope.
  - ii. The team discussed whether lowering the building elevation could reduce the amount of imported fill required. Cochran noted that lowering the building could potentially reduce fill quantities, but

it may also increase the likelihood of encountering rock excavation depending on existing subsurface conditions.

- iii. The District noted that shallow rock is anticipated in portions of the site based on prior site work, tree removal, and observed conditions. The District indicated that rock may be encountered within approximately two to four feet in some areas, with shallower rock anticipated farther down the slope.
  - iv. The team discussed that imported fill is more predictable from a cost standpoint, while rock excavation remains difficult to quantify due to varying rock elevations and irregular formations. Rock encountered during excavation is anticipated to be reused onsite as fill material where practical, reducing both imported fill requirements and offsite disposal.
  - v. Cochran Engineering explained that portions of the proposed site may have limited usable borrow material if rock is encountered too shallow. The preliminary grading approach assumed some material could be obtained from the site, but this assumption needs to be verified before relying on it for bidding.
  - vi. The team discussed whether the drive alignment could be shifted closer to the bus port retaining wall to reduce fill quantities. Cochran noted this could reduce fill but may negatively affect bus maneuverability and the ability to access certain parking spaces in one movement. The District indicated that maintaining adequate bus turning movements remains important.
  - vii. Cochran Engineering will prepare a color-coded cut/fill map identifying areas of anticipated cut and fill, along with recommended test pit / pothole locations.
  - viii. The District will perform potholes / test pits, weather permitting, to determine approximate depth to rock at selected locations.
  - ix. The results of the pothole investigation will be used to refine earthwork assumptions, review potential impacts to the finish floor elevation, and establish appropriate allowances for imported fill, trench rock, and mass rock excavation.
  - x. The team agreed that the current schedule for issuing drawings would not be delayed by the pothole investigation. The drawings may continue toward issuance while additional information is gathered and evaluated.
  - xi. The Design Team noted that the excavation specification will include additional requirements related to earthwork operations, rock excavation, unsuitable materials, and associated coordination procedures to better address potential subsurface conditions encountered during construction.
  - xii. The team discussed the need to establish estimated quantities for rock excavation and other earthwork-related allowances prior to bidding. The cut/fill analysis and pothole investigations will be used to develop appropriate allowance quantities and corresponding unit costs for inclusion in the bid documents.
- b. **Stone Surfacing** – The area identified adjacent to the bus circulation route ("putter area") was reviewed.
- i. The area shall receive a heavier stone surfacing material in lieu of exposed soil.
- c. **Bus Circulation and Stacking** – The team reviewed vehicle circulation entering from Highway FF and evaluated the distance available between the entrance drive and the security gate. Discussion focused on

the number of buses that may reasonably queue at the gate during normal operations and whether modifications to the building location or site layout would be necessary to increase stacking distance.

- i. The District confirmed that only one (1) bus is expected to queue at the gate at any given time under normal operating conditions.
  - ii. Based on the District's operational requirements, the current stacking distance between the entrance drive and gate is considered acceptable.
  - iii. No modifications to the building location or site layout are required to increase bus queuing capacity at this time.
- d. **Asphalt Scope** – The District clarified the intended paving scope associated with the project.
- i. New concrete paving associated with the bus garage and bus port shall be included within the project scope.
  - ii. Asphalt paving beyond the limits identified in the construction documents is not anticipated to be included in the current project scope.
  - iii. The District anticipates completing future asphalt paving improvements under a separate effort.
- e. **Grade Change at Bus Port** – The significant grade change adjacent to the bus port was reviewed. The proposed stair and retaining wall configuration was discussed relative to maintenance access and long-term usability.
- i. District expressed comfort with the proposed grade transition and stair location pending final detailing.
- f. **Southeast Parking Lot Drainage** – Drainage improvements at the southeast corner of the parking lot were reviewed.
- i. A curb inlet is anticipated at the southeast corner of the parking lot.
  - ii. Cochran Engineering to confirm inlet type and final configuration.
- g. **Utility Routing and Directional Boring** – The team reviewed utility routing between the existing school building and the proposed bus garage, including water service, fiber connectivity, and technology infrastructure. Discussion focused on minimizing impacts to the recently resurfaced parking lot, coordinating demolition of existing utility connections, and evaluating directional boring versus open-cut installation methods.
- i. Coordinate the limits of demolition associated with the existing water service.
  - ii. Coordinate routing of the new water service to the bus garage.
  - iii. Coordinate routing requirements for future fiber connectivity and technology infrastructure between the existing school building and the proposed bus garage.
  - iv. Cochran Engineering indicated that directional boring may be comparable in cost, or potentially less expensive, than open-cut installation due to the relatively small water service size.
  - v. The District indicated a preference to avoid saw-cutting the existing parking lot if possible due to the recent overlay and planned seal coat / restriping work.
  - vi. Water service, fiber, and future technology infrastructure should be coordinated to minimize the number of crossings between the existing building and the bus garage.
  - vii. Directional boring remains the preferred approach pending final utility coordination.

- h. **Downspout and Stormwater Strategy** – The team reviewed the proposed downspout discharge approach and opportunities to reduce initial project costs while preserving future flexibility.
  - i. The District indicated a preference for downspouts to discharge at grade during initial construction.
  - ii. The District anticipates installing underground storm piping and final drainage improvements under future work after building construction is complete.
  - iii. Downspouts shall remain separate from retaining wall drainage systems to avoid introducing additional water behind retaining walls.
  - iv. Civil drawings shall accommodate future stormwater connections where practical.
- i. **Parking Stops at Bus Parking** – Wheel stops are not required.
- j. **Parking Lot Striping** – The team reviewed parking lot striping requirements associated with the proposed parking and circulation improvements.
  - i. The District indicated that pavement striping associated with the project parking areas will be completed by the Owner.
  - ii. Parking lot striping is not anticipated to be included within the Contractor's scope of work.
- k. **Site Lighting Coordination** – A discrepancy was identified between drawings regarding an existing site light. One location references relocation while another indicates paving around the existing light standard.
  - i. Coordination required to eliminate conflicting information.
  - ii. The two existing site lights located at the south end of the parking lot shall remain.
  - iii. No new site light is required at the far southeast corner of the parking lot.
  - iv. No modifications to the existing site lighting system are currently anticipated.
- l. **Fencing and Guardrail** – Fencing and guardrail requirements adjacent to the bus port were reviewed.
  - i. Fencing and guardrail are not required along the bus port due to the enclosed nature of the proposed structure.
- m. **Utility and Stormwater Identification** – The District requested additional clarity within the plans.
  - i. Utility and Stormwater components shall be more clearly identified within the plan legends and related documentation.

#### IV. Architectural

- a. **Site Address** – The project address shown in the drawings remains a placeholder. Until a separate address is assigned to the bus garage, the District's existing address of 7466 Highway FF, Lonedell, Missouri shall be used for project coordination and permitting purposes. Any future address assignment will be incorporated into the construction documents as necessary.
- b. **Bus Garage Layout and Equipment Coordination** – The water heater relocation was reviewed. The revised arrangement provides adequate space for an owner-furnished air compressor. Power requirements for the compressor have been coordinated while the compressor and associated piping remain Owner-furnished and Owner-installed.

- i. Current arrangement accepted.
- c. **Interior Liner Panel Height Coordination** – The team reviewed the proposed liner panel heights within the maintenance bays and identified discrepancies between drawing references showing both 8'-0" and 10'-0" liner panel heights.
  - i. The District reviewed the proposed liner panel extents and confirmed a preference for 8'-0" liner panels within the maintenance bays.
  - ii. The Design Team will revise the construction documents to eliminate conflicting information and provide a consistent 8'-0" liner panel height within the maintenance bays.
  - iii. Related details, wall sections, and PEMB coordination information shall be updated accordingly.
- d. **Gate Control Location** – The team reviewed the anticipated gate control location and associated conduit infrastructure required to support the future owner-furnished gate operator and access control system.
  - i. The specific gate operator manufacturer and control equipment will be determined by the District under a separate procurement effort.
  - ii. The construction documents shall identify conduit routing and termination locations necessary to accommodate future gate control equipment.
  - iii. The District will confirm the preferred gate control location to allow final coordination of conduit stub-ups and related infrastructure.
- e. **Snow Guard System** – The snow retention system was revised from a continuous rail assembly to individual snow guard pucks to reduce cost while maintaining performance.
  - i. The district accepted the revised snow guard approach.
  - ii. Snow guards shall be provided at the main building roof areas and above the bus port to help prevent snow and ice from shedding onto pedestrian and vehicle circulation areas.
- f. **Overhead Doors** – The basis-of-design overhead door system was reviewed. The design currently assumes Overhead Door Corporation Model 418 sectional steel doors. Potential impacts associated with alternate manufacturers were discussed, including possible increases in required eave height.
  - i. Basis-of-design manufacturer remains acceptable.
  - ii. The motorized overhead doors shall be provided as a complete system, including operators, controls, and associated accessories required for proper operation. The basis-of-design overhead door package is intended to be all-inclusive and coordinated within the overhead door scope.
  - iii. Overhead door tracks shall follow the roof slope where practical and be coordinated with the PEMB design.
- g. **Bus Port Bird Control** - Interior liner panels within the bus port area were discussed as a method of discouraging bird nesting.
  - i. The District expressed interest in maintaining this option but requested the design team evaluate cost implications.
- h. **Stair Construction** – The current design utilizes cast-in-place concrete stairs with concrete retaining walls and post-mounted handrails. Alternative steel stair construction was discussed.

- i. Concrete stair design remains the preferred solution. Alternate steel construction may be considered by bidders as a value engineering option.
  - i. **Bus Port / PEMB Wall Interface** – The connection between the east PEMB wall, concrete retaining wall, and bus port enclosure was reviewed.
    - i. The Design Team shall provide an enlarged detail illustrating flashing and weatherproofing at the transition between the PEMB wall system and bus port enclosure construction.
- V. **Structural**
  - a. **Bus Port Wall Construction** – The wall assembly above the retaining wall adjacent to the bus port was reviewed. Discussion focused on coordination between the PEMB wall system, liner panels, insulation support, and interior framing requirements.
    - i. Current design intent remains acceptable.
  - b. **PEMB Foundation Coordination & Building Reactions** – Structural foundations remain based upon preliminary loading assumptions pending receipt of final PEMB reactions and manufacturer information.
    - i. Foundation design remains based upon preliminary PEMB reactions.
    - ii. Final PEMB reactions will be required prior to completion of structural design.
    - iii. Structural coordination with the awarded PEMB supplier will occur during the construction administration phase.
    - iv. Foundation revisions may occur following review of PEMB shop drawings and final reaction information.
    - v. The team discussed the importance of obtaining final PEMB reactions as early as possible following project award to avoid delays in structural coordination and fabrication activities.
    - vi. The District requested that critical post-award milestones be clearly identified within the project specifications and contract documents. The Design Team noted that project milestones are already incorporated into the specifications; however, additional emphasis will be placed on timely submission of PEMB design information and other critical coordination items.
    - vii. The need for early submission of PEMB reactions should be emphasized during the preconstruction meeting to facilitate structural coordination and maintain the project schedule.
  - c. **Bus Port Slab Slope** – The team reviewed drainage requirements within the bus port area.
    - i. The bus port slab is intended to slope from west to east to facilitate drainage.
    - ii. Construction documents shall be coordinated to accurately reflect the intended slab slope.
- VI. **Mechanical**
  - a. **Ventilation System** – The ventilation strategy was reviewed, including high exhaust and low intake air distribution intended to improve removal of vehicle exhaust and contaminants.
    - i. Current ventilation approach accepted.
  - b. **Ventilation Controls** – Control methods for the ventilation system were reviewed. The District reiterated a preference for simple operation.

- i. Manual on/off control shall remain.
- c. **Condensing Unit Location** – The exterior condensing unit location and mounting method were reviewed relative to maintenance access, snow accumulation, and adjacent site improvements.
  - i. The condensing unit will be mounted on wall brackets rather than a ground-mounted pad.
  - ii. Wall-mounted installation is intended to elevate the equipment above grade and reduce maintenance concerns associated with snow, ice, and site drainage.
  - iii. Location generally accepted pending final coordination with the PEMB wall system and structural support requirements.
- d. **High Volume Low Speed (HVLS) Fan Coordination** – The location and support requirements for the low-speed fan within the maintenance area were reviewed.
  - i. The anticipated mounting height is generally consistent with the lighting elevation within the maintenance bays.
  - ii. The PEMB supplier shall confirm structural support requirements and acceptability of the proposed fan loading.

## VII. Plumbing

- a. **Service Sink Selection** – The District reviewed the proposed wall-mounted service sink and associated FRP wall protection.
  - i. Wall-mounted service sink accepted as the preferred solution.
- b. **Water Heater Disconnect** - The District requested clarification regarding the disconnect shown adjacent to the water heater. The electrical engineer confirmed the device is a fused disconnect switch serving the water heater.
  - i. No design changes required.
- c. **Septic Tank, Grinder Pump & Electrical Coordination** – The team reviewed the proposed sanitary connection serving the bus garage. The District clarified that a septic tank is required ahead of the existing treatment system and grinder station. The location of the septic tank, grinder pit, and associated electrical service were discussed to ensure compatibility with the District's existing wastewater system and future maintenance requirements.
  - i. The team confirmed that a septic tank shall be provided as part of the project upstream of the grinder station prior to discharging into the District's existing treatment system.  
[Revised Note 06/11/2026].
  - ii. Cochran Engineering is finalizing the grinder pump sizing and indicated the pump will likely be in the range of approximately 2 horsepower.
  - iii. The electrical engineer requested final pump horsepower information to confirm conductor sizing, overcurrent protection, and electrical service requirements.
  - iv. The project electrical service was confirmed as 120/208V, 3-phase, 4-wire, and the grinder pump selection shall be coordinated with the available service.

- v. The team discussed the likelihood of providing dedicated electrical circuits for the grinder pump and associated high-level alarm equipment, pending final equipment selection and manufacturer requirements.
- vi. The District noted that typical grinder station installations include a high-level alarm and associated controls. Final electrical requirements will be coordinated upon receipt of manufacturer data and pump specifications.

## VIII. Electrical

- a. **Office Lighting Controls** – The District reviewed switching arrangements for the office area. The electrical engineer confirmed revisions had already been made to provide three-way switching at both office entrances for improved usability.
  - i. Three-way switching at both office entrances accepted.
- b. **Utility Room Switching** – Switch locations serving the utility/storage area were reviewed and simplified to eliminate unnecessary switches while maintaining accessibility.
  - i. Revised switching arrangement accepted.
- c. **Toilet Room Controls** – The District reviewed operation of the toilet room lighting and exhaust fan controls.
  - i. Lighting and exhaust fan shall remain independently switched.
- d. **Maintenance Bay Receptacles** – Electrical receptacle locations within the maintenance bays were reviewed.
  - i. Receptacles located within the maintenance bays shall be mounted at 48 inches above finished floor.
- e. **Bus Port Conduit Routing and Concealment** – The team reviewed conduit routing within the bus port and retaining wall areas. The District expressed a preference to avoid exposed conduit and electrical boxes where practical.
  - i. Electrical conduit and junction boxes serving the bus port lighting and related systems shall be concealed within walls, retaining wall construction, or other concealed locations where feasible.
  - ii. Exposed conduit installations should be minimized and coordinated with the architectural design to maintain a clean appearance.
  - iii. The proposed concealed conduit routing approach was accepted by the District.
- f. **Exterior Lighting** – The team reviewed exterior lighting locations, including wall packs and flood lighting at the bus port. Potential conflicts between lighting fixtures and PEMB framing were discussed. Lighting controls and fixture locations were reviewed.
  - i. Exterior wall pack relocation is acceptable.
  - ii. Flood lighting at the bus port is acceptable.
  - iii. The proposed street-side wall pack was reviewed for perimeter security lighting.
  - iv. Exterior lighting shall remain photocell controlled.

- v. The design currently utilizes a common photocell control strategy for exterior building and bus port lighting. A photocell/time clock combination and bus port lighting override options were discussed; however, no modifications to the proposed control strategy were requested.
- g. **Fire Alarm Requirements** – The team discussed whether a fire alarm system would be required for the proposed bus garage based on the anticipated occupancy classification and building size.
  - i. Based on the current building configuration and occupancy classification, the design team does not anticipate a fire alarm system will be required.
  - ii. The project will continue without a fire alarm system unless required by the Authority Having Jurisdiction during permit review.
- h. **Access Control Rough-In** – Door security requirements were reviewed.
  - i. Access control rough-in shall be provided at the north/front entrance door.
  - ii. Access control rough-in is not required at the south entrance door.

#### **IX. Project Budgeting, Risk Management & Allowances**

- a. **Earthwork and Rock Excavation Risk Assessment** – The team discussed the uncertainty associated with balancing imported fill quantities against potential rock excavation throughout the site. Significant discussion focused on developing a strategy that would allow the project to proceed toward bidding while appropriately accounting for subsurface conditions that cannot be fully verified prior to construction.
  - i. The team acknowledged that imported fill quantities can generally be estimated with greater certainty than rock excavation quantities.
  - ii. Rock excavation remains difficult to predict due to varying rock elevations, irregular formations, and limited subsurface data available across the entire project site.
  - iii. Additional field investigations, including potholes and test pits, were discussed as a means of improving the design team's understanding of existing conditions; however, all parties acknowledged that even extensive testing would not eliminate uncertainty associated with rock excavation.
  - iv. The District, Design Team, and Civil Engineer discussed utilizing bidding allowances to manage uncertainty associated with earthwork, trench rock, and potential mass rock excavation.
  - v. Information obtained from the proposed cut/fill analysis and field investigations will be used to refine allowance quantities and establish a more reliable project budget prior to bidding.
  - vi. The team agreed that development of reasonable allowances represents the most practical approach to managing subsurface risk while maintaining the current project schedule.
  - vii. The team acknowledged that final earthwork costs may vary depending upon actual subsurface conditions encountered during construction and that bidding allowances are intended to reduce the owner's exposure to unknown site conditions.

#### **X. Project Schedule / Procurement**

- a. **Drawing Issuance Schedule** – The team reviewed the impact of additional grading analysis and test pit investigations on the project schedule.
  - i. The June 10, 2026 drawing issuance date remains the current target.
  - ii. The cut/fill analysis and pothole investigations will not delay issuance of the bid documents.
  - iii. Allowances may be adjusted as additional subsurface information becomes available.
  
- b. **Bidding Schedule** – The District discussed upcoming Board meeting dates and potential timing for project approval and bidding activities. June 29, 2026 was identified as the preferred date for Board action if available.
  - i. Design team will continue advancing documents toward bidding while monitoring Board scheduling.
  
- c. **Contractor Interest and Procurement Process** – The design team reviewed the anticipated contractor solicitation process, including electronic distribution of project information and tracking of contractor interest.
  - i. The District requested that Total Building Solutions, GS&S Construction, and ICS Construction Services be included in the project bid distribution and notified when bidding documents become available.
  - ii. The District discussed additional contractor interest in the project. Poettker Construction was identified as a contractor that had expressed interest in the project, and the design team noted that the firm was already included within the contractor outreach and solicitation efforts.
  - iii. The design team will incorporate the requested contractors into the project solicitation and plan holder outreach efforts.
  
- d. **Pre-Bid Meeting Coordination** – The team discussed anticipated pre-bid meeting logistics and contractor access to the project site.
  - i. The meeting is anticipated on June 17, 2026 at 1:30 p.m., pending final confirmation.
  - ii. Contractors shall meet at the District front office prior to proceeding to the project site for the pre-bid meeting.

## XI. Meeting Summary

- a. The District and design team completed a page-turn review of the 95% Construction Document set. Major discussion items included earthwork quantities, rock excavation risk, utility routing, site circulation, bus port grading, building systems coordination, and preparation of bidding documents. The project remains on track for the anticipated June 10, 2026 document issuance, subject to completion of the remaining coordination items identified during the meeting.

## XII. Drawing Review Comments

- a. Additional drawing review comments were documented directly on the marked-up review set and are incorporated herein by reference. The Design Team will review and incorporate the marked-up drawing comments as appropriate during final document coordination.

### **XIII. Next Steps & Action Items**

- a. Cochran Engineering:
  - i. Prepare and distribute cut / fill exhibit showing anticipated grading conditions.
  - ii. Review pothole information and refine grading assumptions.
  - iii. Coordinate final grinder pump specifications, high-level alarm requirements, electrical service requirements, conductor sizing, and overcurrent protection requirements after final pump selection.
- b. Hoener Architects:
  - i. Coordinate final architectural and structural specifications.
  - ii. Resolve drawing conflicts regarding existing site light relocation.
  - iii. Evaluate liner panels at bus port as a potential alternate.
  - iv. Add Total Building Solutions, GS&S Construction, and ICS Construction Services to the project bid distribution list.
  - v. Provide the pre-engineered metal building (PEMB) specification and basis-of-design information to the District for review prior to bidding.
- c. ASDG, LLC:
  - i. Continue PEMB reaction and foundation coordination after the project is awarded.
- d. ECS:
  - i. Mechanical engineer to finalize the condensing unit installation requirements.
  - ii. Electrical engineer to incorporate switching revisions into final documents.
- e. Design Team:
  - i. Evaluate impacts to finish floor elevation based on revised earthwork analysis.
  - ii. Continue development of rock excavation and earthwork allowances for bidding purposes.
  - iii. Revise construction documents to provide a consistent 8'-0" liner panel height within the maintenance bays and eliminate conflicting references in the drawings.
  - iv. Complete final document coordination and issue bid-ready set.
  - v. Schedule pre-bid meeting (potential 06/17/2026)
  - vi. Develop estimated rock excavation quantities and corresponding allowance language for inclusion in the bidding documents following review of the cut/fill analysis and pothole investigation results.
- f. District:
  - i. Perform potholes / test pits and document depth-to-rock findings.
  - ii. Confirm final gate control location.
  - iii. Confirm final project address once assigned.

**End of Meeting Notes**

# THE PROPOSED BUILDING FOR: LONEDELL BUS GARAGE

LONEDELL R-XIV SCHOOL DISTRICT

LONEDELL, MISSOURI

**PROJECT NO.: 25-43B**

**DATE ISSUED 06.04.2026**

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**GENERAL DRAWINGS**

CA1.1 CODE ANALYSIS PLAN

**CIVIL DRAWINGS**

T1 TOPOGRAPHIC SURVEY  
C1 DEMOLITION PLAN  
C2 SITE PLAN  
C3 GRADING PLAN  
C4 UTILITY PLAN  
D1 DETAIL SHEET  
D2 DETAIL SHEET

**ARCHITECTURAL DRAWINGS**

A1.1 ARCHITECTURAL SITE PLAN  
A1.2 FLOOR PLAN  
A1.3 ROOF PLAN  
A2.1 BUILDING ELEVATIONS  
A2.2 BUILDING ELEVATION, BUILDING SECTION AND EXTERIOR STAIR  
A2.3 BUILDING SECTIONS  
A3.1 LARGE SCALE PLANS AND DETAILS  
A4.1 WALL SECTIONS  
A4.2 WALL SECTIONS  
A5.1 REFLECTED CEILING PLAN

**STRUCTURAL DRAWINGS**

S1.1 GENERAL NOTES  
S1.2 SPECIAL INSPECTION TABLES  
S1.3 TYPICAL DETAILS  
S1.4 TYPICAL DETAILS  
S2.1 FOUNDATION PLAN  
S3.1 CONCRETE COLUMN DETAILS  
S4.1 FOUNDATION DETAILS

**MECHANICAL HVAC DRAWINGS**

M2.01 FLOOR PLAN - MECHANICAL

**PLUMBING DRAWINGS**

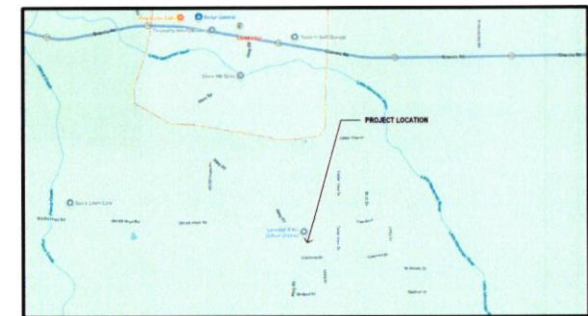
P2.01 UNDER FLOOR PLAN - PLUMBING  
P2.02 FLOOR PLAN - PLUMBING  
P3.01 SCHEDULES & DETAILS

**ELECTRICAL DRAWINGS**

E2.01 FLOOR PLAN - ELECTRICAL  
E3.01 REFLECTED CEILING PLAN - ELECTRICAL  
E4.01 ELECTRICAL ONE-LINE DIAGRAM, PANEL SCHEDULE, DETAILS  
E4.02 TELECOMMUNICATIONS DETAILS

**GENERAL CODE COMPLIANCE**

PROJECT NAME: THE PROPOSED BUILDING FOR LONEDELL BUS GARAGE  
PROJECT LOCATION: LONEDELL R-14 SCHOOL DISTRICT  
746 HWY FF  
LONEDELL, MO 63096  
PROJECT DESCRIPTION: PRE-ENGINEERED METAL BUILDING FOR BUS GARAGE AND BUS PORT  
BUILDING JURISDICTION: CITY OF LONEDELL  
FIRE SUPPRESSION: NON-SPRINKLERED  
BUILDING CODES IN EFFECT:  
2021 INTERNATIONAL BUILDING CODE  
2021 INTERNATIONAL MECHANICAL CODE  
2021 INTERNATIONAL PLUMBING CODE  
2021 NATIONAL ELECTRIC CODE  
BUILDING CODE INFORMATION:  
TYPE OF CONSTRUCTION: I-B (NON-COMBUSTIBLE)  
OCCUPANCY CLASSIFICATION:  
USE GROUP: S-2 (ON-WHEEL STORAGE GARAGES WITH ACCESSORY GROUP: B OFFICE/ADMIN AREA)  
TOTAL BUILDING AREA: 1,628 S.F.  
SEE CAL 1 FOR CODE ANALYSIS PLAN INFORMATION



**LOCATION MAP**  
NOT TO SCALE



**ALTERNATE**

1A - PREFINISHED INTERIOR LINER PANELS SYSTEM IN MAINTENANCE AREA.  
PROVIDE PREFINISHED 10'-0" TALL METAL LINER PANELS AT THE FULL INTERIOR PERIMETER OF THE MAINTENANCE GARAGE AREA. PROVIDE 8" FIRE-RETARDANT TREATED PLYWOOD SHEATHING BEHIND THE LINER PANEL SYSTEM. LINER PANEL SYSTEM SHALL BE INSTALLED OVER THE BASE METAL BUILDING AND INSULATION SYSTEM AND IN ACCORDANCE WITH THE PERM MANUFACTURER'S RECOMMENDATIONS.  
BASE BID SHALL INCLUDE EXPOSED VINYL-FACED METAL BUILDING INSULATION AT THE PERIMETER WALLS OF THE MAINTENANCE GARAGE AREA.  
1B - PREFINISHED INTERIOR LINER PANELS SYSTEM IN BUS PARKING BUS PORT.  
PROVIDE PREFINISHED 8'-0" - 12'-0" TALL METAL LINER PANELS AT THE INTERIOR PERIMETER OF THE BUS PARKING BUS PORT AREA AND UNDER THE ROOF TYPICAL LINER PANEL SYSTEM SHALL BE INSTALLED OVER THE BASE METAL BUILDING DIRECTLY OVER WALL GIRTS AND UNDER ROOF PURLINS AND IN ACCORDANCE WITH THE PERM MANUFACTURER'S RECOMMENDATIONS.  
BASE BID SHALL INCLUDE EXPOSED METAL BUILDING PERIMETER WALL GIRTS AND UNDER ROOF PURLINS OF THE BUS PARKING BUS PORT.

8'-0" HEIGHT IS ACCEPTABLE

PRELIMINARY  
NOT FOR  
CONSTRUCTION  
99%  
PRESS

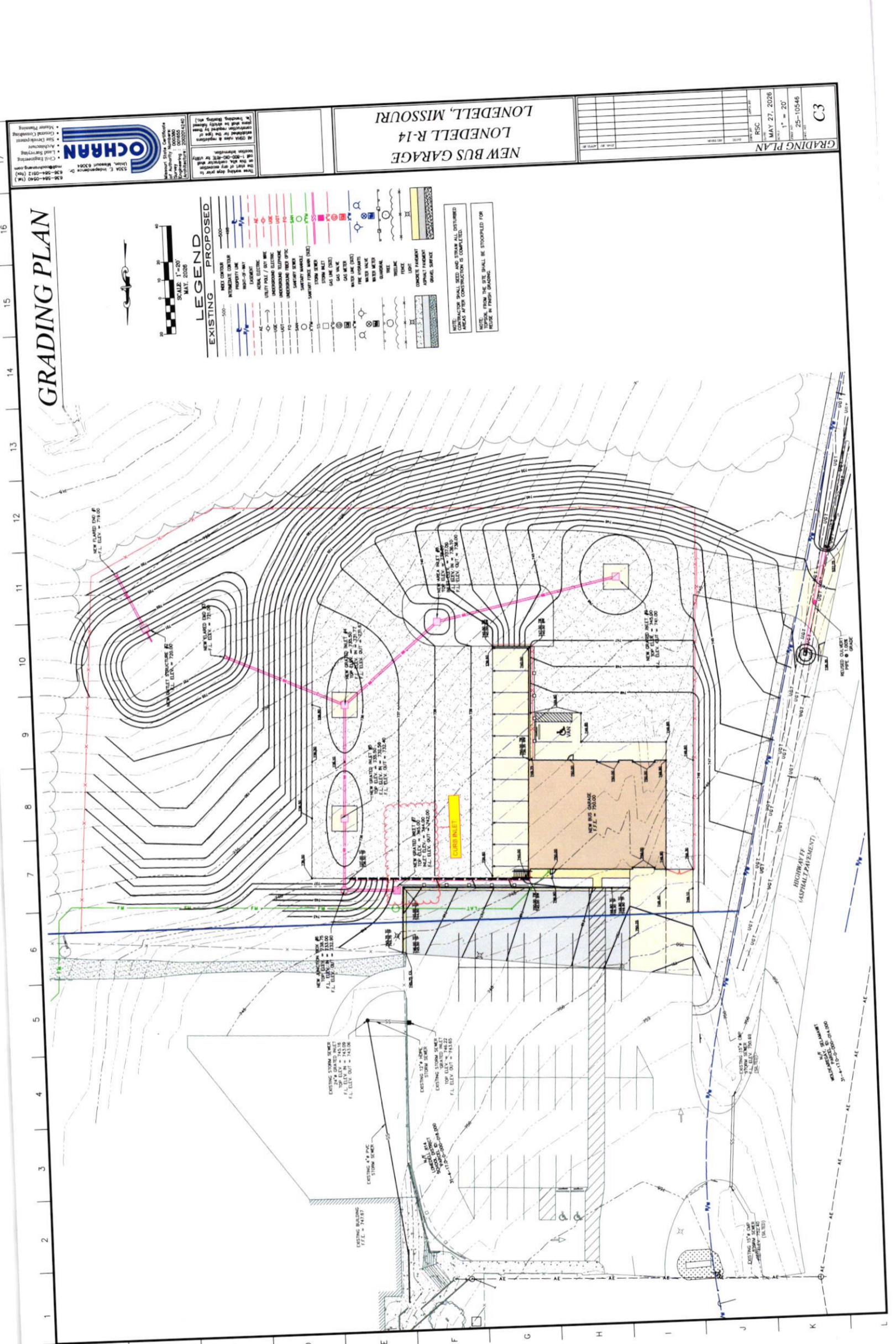
Set No.:











**CH2M HILL OCHRRAN**  
 530A E Independence Dr  
 Independence, MO 64050  
 816-352-0942  
 816-352-0943 (Fax)  
 816-352-0944 (Cell)  
 816-352-0945 (Home)  
 816-352-0946 (Mobile)  
 816-352-0947 (Pager)  
 816-352-0948 (Fax)  
 816-352-0949 (Fax)  
 816-352-0950 (Fax)  
 816-352-0951 (Fax)  
 816-352-0952 (Fax)  
 816-352-0953 (Fax)  
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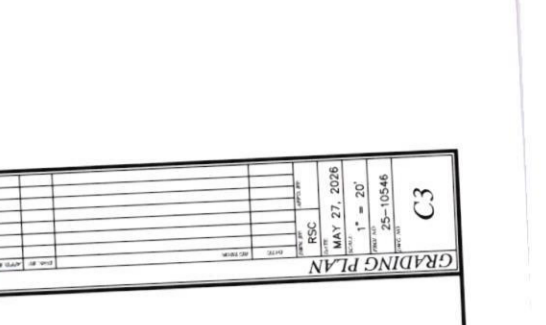
**NEW BUS GARAGE**  
**LONDELL R-14**  
**LONDELL, MISSOURI**

**GRADING PLAN**  
 RSC  
 MAY 27, 2026  
 1" = 20'  
 25-10546  
 C3

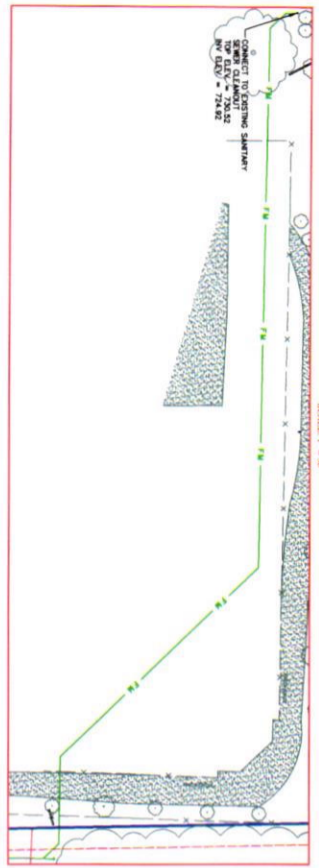
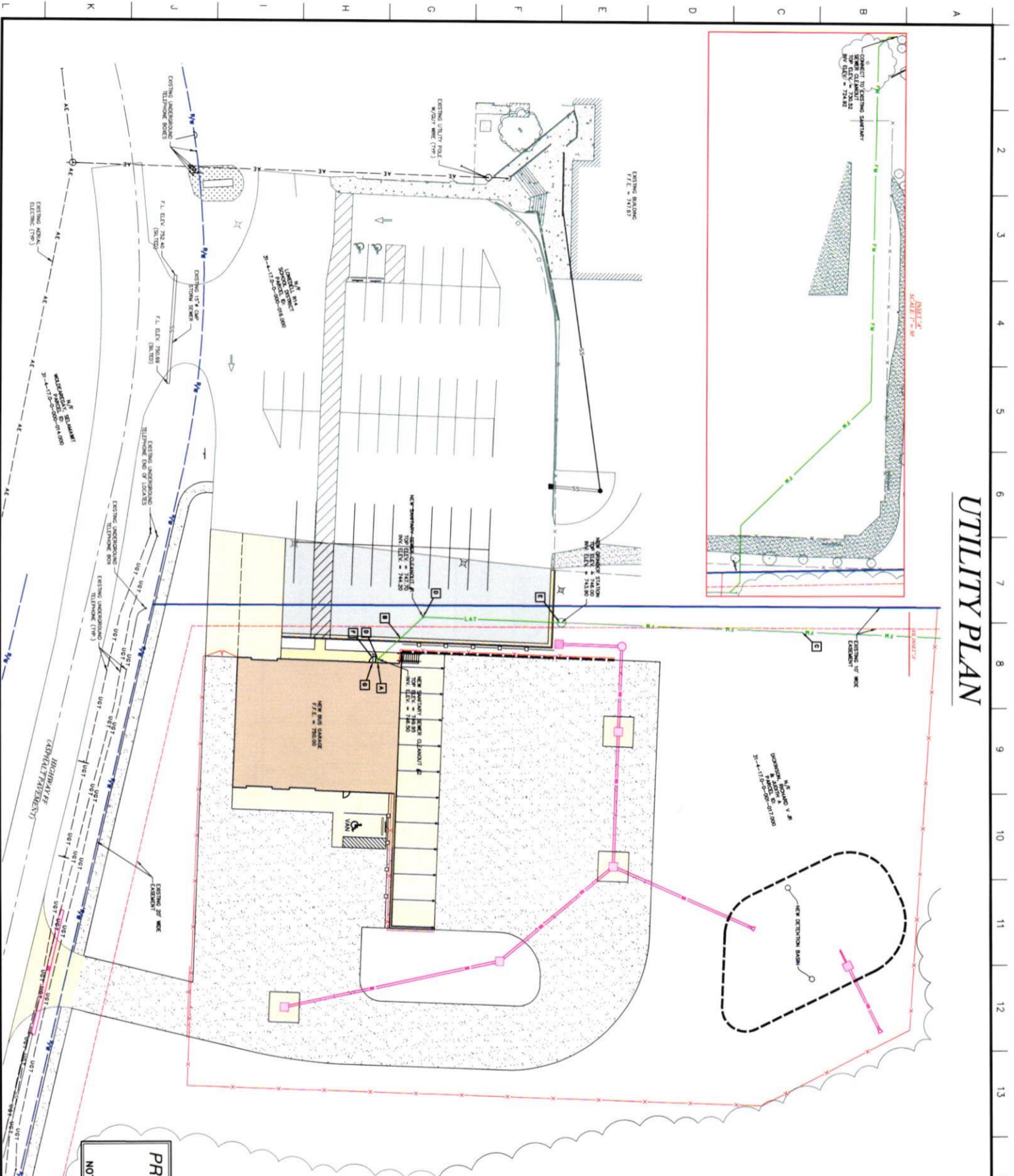
**LEGEND**  
**EXISTING**  
 1" = 20'  
 MAY, 2026

**PROPOSED**  
 1" = 20'  
 MAY, 2026

NOTE: ALL UTILITIES SHALL BE DELETED AND REINSTALLED AS SHOWN ON THIS PLAN AFTER CONSTRUCTION IS COMPLETED.  
 NOTE: FINISH ELEVATIONS SHALL BE SHOWN IN FINISH GRADING.



# UTILITY PLAN



NEW 12" V.8  
CONCRETE MANHOLE &  
VAULT  
3'-x-11'-0" (2025) (17100)



## LEGEND

EXISTING	PROPOSED
3000	3000
3001	3001
3002	3002
3003	3003
3004	3004
3005	3005
3006	3006
3007	3007
3008	3008
3009	3009
3010	3010
3011	3011
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3049	3049
3050	3050

## UTILITY LEGEND

- A NEW 4" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- B NEW 6" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- C NEW 8" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- D NEW 12" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- E NEW 18" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- F NEW 24" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- G NEW 30" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- H NEW 36" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- I NEW 42" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- J NEW 48" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- K NEW 54" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- L NEW 60" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- M NEW 66" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- N NEW 72" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- O NEW 78" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- P NEW 84" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- Q NEW 90" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- R NEW 96" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- S NEW 102" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- T NEW 108" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- U NEW 114" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- V NEW 120" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- W NEW 126" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- X NEW 132" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- Y NEW 138" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- Z NEW 144" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AA NEW 150" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AB NEW 156" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AC NEW 162" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AD NEW 168" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AE NEW 174" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AF NEW 180" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AG NEW 186" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AH NEW 192" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AI NEW 198" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AJ NEW 204" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AK NEW 210" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AL NEW 216" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AM NEW 222" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AN NEW 228" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AO NEW 234" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AP NEW 240" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AQ NEW 246" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AR NEW 252" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AS NEW 258" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AT NEW 264" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AU NEW 270" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AV NEW 276" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AW NEW 282" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AX NEW 288" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AY NEW 294" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS
- AZ NEW 300" SANITARY SEWER SERVICE CHUTE. SEE AWP PLANS

**PRELIMINARY DRAWING**  
FOR REVIEW PURPOSES ONLY  
NOT TO BE USED FOR CONSTRUCTION

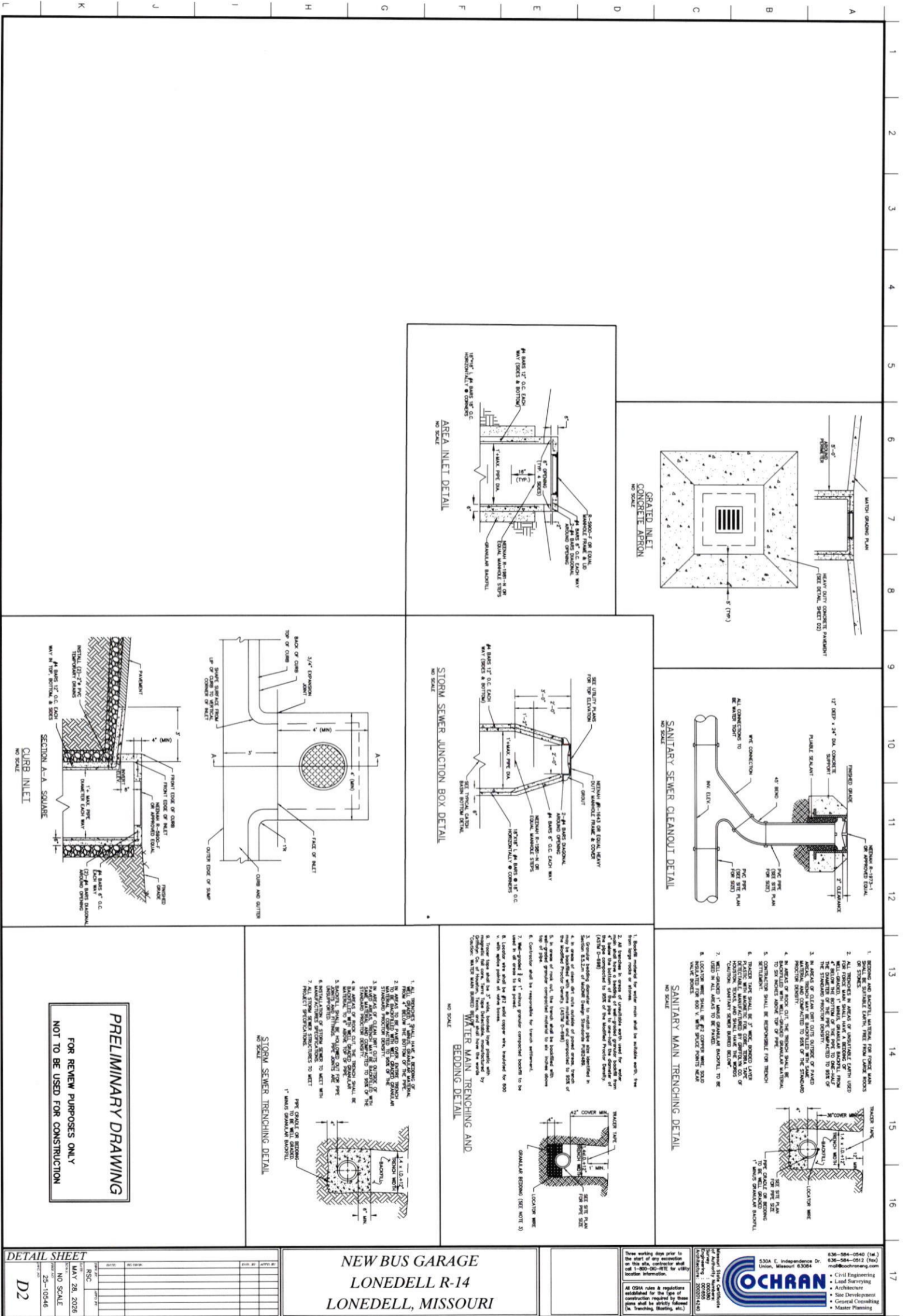
<p><b>UTILITY PLAN</b></p> <p>C4</p> <p>SCALE 1" = 20'</p> <p>MAY 28, 2026</p>	<p><b>NEW BUS GARAGE</b> <b>LONEDELL R-14</b> <b>LONEDELL, MISSOURI</b></p>	<p>These working drawings are prepared in accordance with the standards and specifications of the Missouri State Board of Professional Engineers and Architects, Inc. (MSBEA) and the Missouri State Board of Professional Surveyors, Inc. (MSBSI). All work shall be in accordance with the applicable codes and regulations of the State of Missouri and the local jurisdiction. The contractor shall be responsible for obtaining all necessary permits and for complying with all applicable laws and regulations. The contractor shall be responsible for the accuracy of the field data and for the proper installation and maintenance of the utility system. The contractor shall be responsible for the safety of the construction site and for the protection of the existing utility system. The contractor shall be responsible for the cleanup and restoration of the site after construction is complete. The contractor shall be responsible for the payment of all taxes and fees associated with the project. The contractor shall be responsible for the insurance and bonding required for the project. The contractor shall be responsible for the coordination of the project with all other utility owners and agencies. The contractor shall be responsible for the communication of the project progress to the owner and the design team. The contractor shall be responsible for the completion of the project within the agreed-upon schedule and budget. The contractor shall be responsible for the maintenance and repair of the utility system for the life of the project. The contractor shall be responsible for the training and education of the utility workers. The contractor shall be responsible for the research and development of new utility technologies. The contractor shall be responsible for the innovation and improvement of the utility system. The contractor shall be responsible for the sustainability and environmental protection of the utility system. The contractor shall be responsible for the social and economic benefits of the utility system. The contractor shall be responsible for the overall success of the utility system.</p>
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**OCHRAN**

Professional State Certificate  
Professional Engineer  
Professional Surveyor  
Professional Architect  
Professional Planner

530A E. Independence Dr.  
Union, Missouri 63084  
636-584-0540 (tel.)  
636-584-0912 (fax)  
mo@ochran.com







**GENERAL NOTES**

1. ARCHITECTURAL SITE PLAN IS FOR ARCHITECTURAL REFERENCES ONLY.
2. BUILDING FINISH FLOOR ELEVATIONS: 750.00 (CIVIL), 100'-0" (ARCHITECTURAL)
3. SEE CIVIL, MECHANICAL, ELECTRICAL, & PLUMBING DRAWINGS FOR PRIMARY SITE INFORMATION INCLUDING UTILITY LOCATIONS AND SITE UTILITY WORK, PARKING, PAVING, LANDSCAPING, ETC.
4. PAINT ALL DIRECTIONAL ARROWS, LANE MARKINGS, PARKING STRIPES, AND CROSSWALKS. SEE CDR, DRAWINGS FOR LOCATIONS AND LAYOUT (SEE SPECS FOR PAINT FINISH)

G.C. TO COORDINATE WITH CIVIL, STRUCTURAL, AND MEP DRAWINGS FOR DEMOLITION AND NEW WORK NOT SHOWN ON THIS PLAN

**SITE PLAN KEYED NOTES**

1. NO WORK UNLESS NOTED OTHERWISE.
2. EXISTING PROPERTY LINE.
3. EXISTING PARKING LOT PAVING AND STRIPING SHALL REMAIN. MODIFY AS NEEDED TO ACCEPT THE NEW CONSTRUCTION.
4. NEW ASPHALT PAVING, SEE CIVIL DRAWINGS.
5. NEW GRAVEL PARKING LOT AND DRIVES, SEE CIVIL DRAWINGS.
6. PAINT NEW PARKING LOT STRIPING (10' X 20'), SEE SPECS.
7. NEW CONCRETE PAVING, SEE CIVIL DRAWINGS.
8. WARP NEW GRAVEL PARKING LOT AS NEEDED TO MEET THE ADA PARKING SPOT SLOPING AT 1/4" = 1'-0". SEE CIVIL DRAWINGS.
9. NEW CONCRETE PAVING - SLOPE AWAY FROM BUILDING / CONCRETE RETAINING WALL. SEE STRUCTURAL DRAWINGS.
10. NEW CONCRETE SIDEWALK - SLOPE AWAY FROM BUILDING / CONCRETE RETAINING WALL. SEE CIVIL DRAWINGS AND DA 1 / 3.01.
11. PAINT BUS PARKING STRIPING (12' X 30'). SEE SPECS.
12. NEW PARKING BUMPER, SEE CIVIL DRAWINGS.
13. PAINT NEW ADA PARKING SYMBOL. SEE DA 2 / 10.16.
14. INSTALL NEW ADA PARKING SIGNAGE ON WALL. SEE DA 1 / 10.16.
15. NEW 6'-0" HIGH CHAINLINK FENCE BY SCHOOL DISTRICT.
16. NEW 6'-0" HIGH CHAINLINK SWING GATES BY SCHOOL DISTRICT.
17. NEW 6'-0" HIGH CHAINLINK SLIDE GATE WITH MOTORIZED SYSTEM AND KEYPAD POST. SEE ELECTRICAL DRAWINGS. BUSES TO HAVE REMOTES.
18. DETENTION POND. SEE CIVIL DRAWINGS.
19. GRASS SEED AND HAY ALL DISTURBED AREAS.
20. EDGE OF EXISTING TREE LINE. SEE CIVIL DRAWINGS.
21. BUS SHOWN, NOT IN CONTRACT.
22. CONCRETE RETAINING WALL - SEE STRUCTURAL DRAWINGS.
23. EXISTING LIGHT STANDARD SHALL REMAIN. PROTECT FROM DAMAGE DURING DEMOLITION AND RENOVATION. LOCATION MAY VARY.
24. CONDENSING UNIT - SEE MECHANICAL DRAWINGS.

PRELIMINARY  
NOT FOR  
CONSTRUCTION  
85% PROGRESS  
SET

MIKE HIMES  
LICENSE #A2004016308  
ARCHITECTURAL CORP.  
NO. 000193

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MEP Engineer  
ECS CONSULTING ENGINEERS  
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CONSULTING ENGINEERS  
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Structural Engineer  
ARCO, LLC  
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ST. LOUIS, MISSOURI 63109  
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Civil Engineer  
CONSULTING ENGINEERS  
LONEDELL, MISSOURI  
1100 S. MAIN ST.  
ST. LOUIS, MO 63102  
771.643.2122

THE PROPOSED BUILDING FOR:  
**LONEDELL BUS GARAGE**  
LONEDELL R-VIIV SCHOOL DISTRICT  
**HOENER ARCHITECTS, LLC**  
6707 PLAINVIEW AVENUE  
ST. LOUIS, MISSOURI 63109  
OFFICE: 314. 781.9855  
hoenerarchitects.com



ALL DIMENSIONS MUST BE  
VERIFIED AT BUILDING BE-  
FORE WORK IS EXECUTED.  
THIS DRAWING IS THE PRO-  
PERTY OF THE ARCHITECTS  
AND SHALL NOT BE COPIED  
OR DUPLICATED WITHOUT  
THEIR CONSENT.

PROJ. NO. 25-43B

DATE ISSUED

06.04.2026

DATE REVISION

DRAWN BY Author

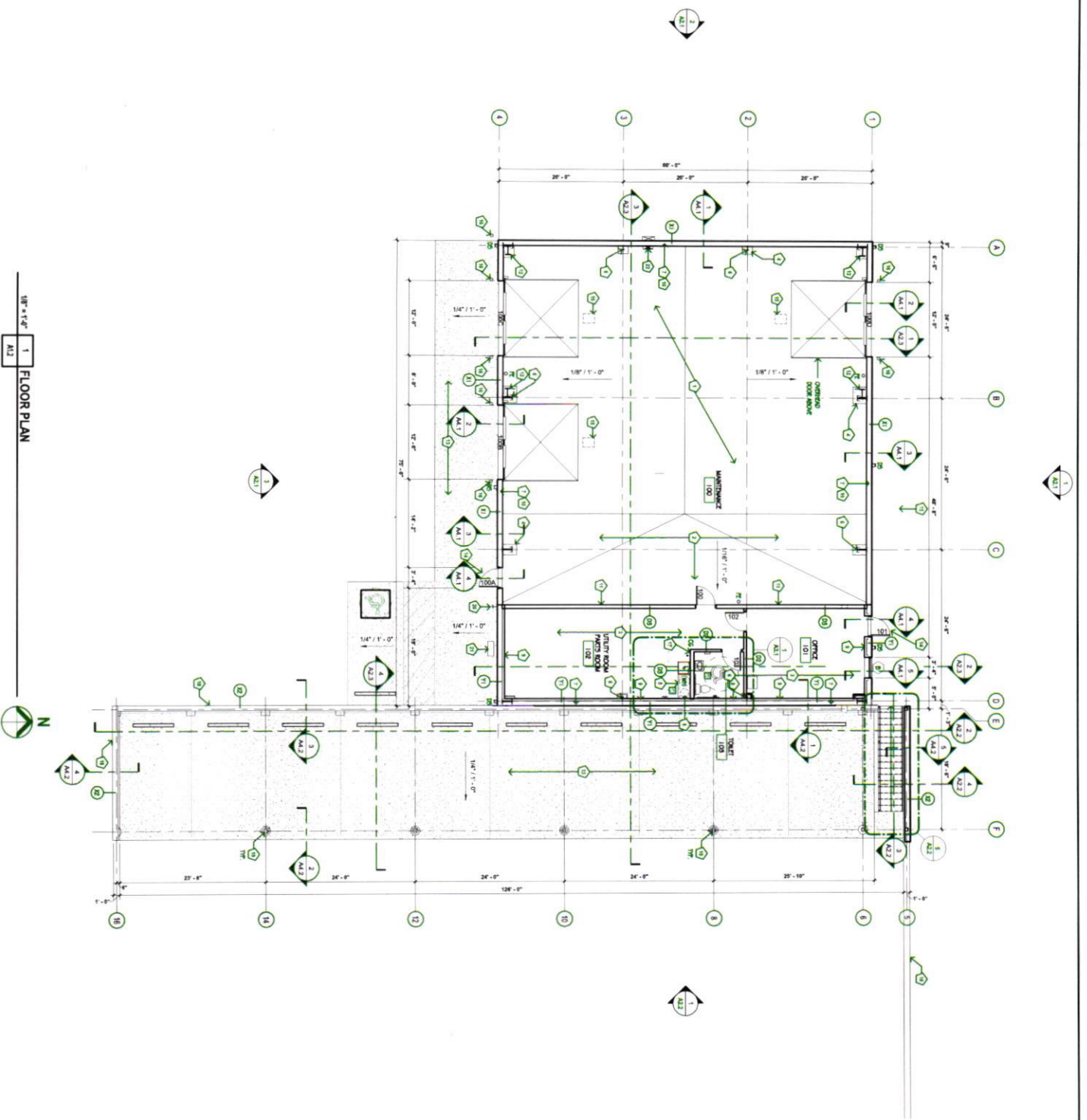
CHECKED BY MW

SHEET NO.

**A1.1**

1/8" = 1'-0" ARCHITECTURAL SITE PLAN





1/8" = 1'-0" 1 FLOOR PLAN A1.2

**FLOOR PLAN LEGEND**

- C.A. CORNER QUARTER SET BRICK
- D.A. EXTERIOR DOOR WITH GLASS INSERT
- E.A. EXTERIOR DOOR WITH GLASS INSERT AND TRANSOM
- F.A. FLOOR DOOR WITH GLASS INSERT
- G.A. GROUND LEVEL
- H.A. HOSE RINK
- I.A. INTERIOR DOOR WITH GLASS INSERT
- J.A. JUNCTION OF WALLS
- K.A. KITCHEN
- L.A. LIVING ROOM
- M.A. MECHANICAL ROOM
- N.A. NESTING
- O.A. OFFICE
- P.A. PAVEMENT
- Q.A. QUARTER
- R.A. RESTROOM
- S.A. SERVICE
- T.A. TYPICAL
- U.A. UTILITY ROOM
- V.A. VESTIBULE
- W.A. WATERCLOSET
- X.A. WINDOW ACCESSORY
- Y.A. YARD
- Z.A. ZONE

**FLOOR PLAN GENERAL NOTES**

1. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR THE FOLLOWING:
  1. STEEL BEAM FRAME WALLS, STEEL COLUMN WALLS, VAPOR BARRIER, WALL/DOOR FINISHES AND FINISH MANUFACTURES.
  2. ALL FLOOR FINISHES SHALL BE 1/2" MINIMUM.
  3. ALL FLOOR FINISHES SHALL BE 1/2" MINIMUM.
  4. ALL FLOOR FINISHES SHALL BE 1/2" MINIMUM.
  5. SEE ROOF PLAN FOR ROOF FINISHES.
  6. SEE ROOF PLAN FOR ROOF FINISHES.
  7. SEE ROOF PLAN FOR ROOF FINISHES.
  8. SEE ROOF PLAN FOR ROOF FINISHES.
  9. SEE ROOF PLAN FOR ROOF FINISHES.
  10. SEE ROOF PLAN FOR ROOF FINISHES.
  11. SEE ROOF PLAN FOR ROOF FINISHES.
  12. SEE ROOF PLAN FOR ROOF FINISHES.

**FLOOR PLAN KEYED NOTES**

1. 4" THICK CONCRETE FLOOR SLAB OVER 1/2" MIN. VAPOR BARRIER AND GRANULAR FILL. WOOD BRIDGES SHALL BE INSTALLED OVER ALL JOINTS. PROVIDE FINISH FLOOR FINISH.
2. 4" THICK CONCRETE FLOOR SLAB OVER 1/2" MIN. VAPOR BARRIER AND GRANULAR FILL. WOOD BRIDGES SHALL BE INSTALLED OVER ALL JOINTS. PROVIDE FINISH FLOOR FINISH.
3. 4" THICK CONCRETE FLOOR SLAB OVER 1/2" MIN. VAPOR BARRIER AND GRANULAR FILL. WOOD BRIDGES SHALL BE INSTALLED OVER ALL JOINTS. PROVIDE FINISH FLOOR FINISH.
4. 4" THICK CONCRETE FLOOR SLAB OVER 1/2" MIN. VAPOR BARRIER AND GRANULAR FILL. WOOD BRIDGES SHALL BE INSTALLED OVER ALL JOINTS. PROVIDE FINISH FLOOR FINISH.
5. 4" HIGH TOP OF CONCRETE MAINTENANCE SLAB AT WATER HEATERS AND COMPRESSOR.
6. STEEL BEAM FRAME MEMBERS, STEEL COLUMNS - STEEL STRUCTURE TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER. BOTTOM OF BEAM FRAME SHALL REST ON TOP OF THE 4" HIGH FOUNDATION. TYP.
7. WALL GUTTS, ROOF FLASHING, WALL / ROOF INSULATION AND PREFINISHED METAL SIDING TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
8. COMPRESSOR BY THE OWNER. SEE ELECTRICAL DRAWINGS.
9. PROVIDE 6" COPING BOARD OVER METAL STEEL FLASHING. (1) 1/2" MIN. AT 1/2" O.C. UP TO 6" ABOVE CEILING PLANE BY CONTRACTOR - TYP.
10. PROVIDE PREFINISHED LINER PANELS OVER INTERIOR OF WALL GUTTS 4" A.F.F. TYP.
11. PROVIDE PREFINISHED LINER PANELS OVER 1/2" PRE-CAST RETARDANT FIBERWOOD ABOVE AND SEAL TIGHT FOR FIRE RATING.
12. LOCATION OF PORTAL FRAME BRACING IN THE WALL.
13. CONCRETE EXTERIOR FINISH / SIDEWALK. SEE SITE PLAN FOR MORE INFORMATION.
14. CONCRETE DOOR STOOP OVER LEAN CONCRETE. SLOPE 1/4" PER FOOT AWAY FROM BUILDING. SEE DA 3.11.
15. INSTALL MOTORIZED OVERHEAD DOOR OPERATOR AS INDICATED. SEE ELECTRICAL DRAWINGS AND SCHEDULE.
16. EXTERIOR PIPE DOLLARD - PAINT. SEE DA DETAIL 1.15.1.
17. PROVIDE 4" SIP WALL PROTECTION BOARD BEHIND THE SERVICE SIGN FROM CORNER GUARD TO INSIDE CORNER.
18. CONCRETE RETAINING WALL. SEE STRUCTURAL DRAWINGS.
19. STEEL COLUMNS - PAINT. COLUMNS AND STEEL STRUCTURE TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
20. HOSE RINK - SEE MECHANICAL DRAWINGS.
21. CONDENSING UNIT - SEE MECHANICAL DRAWINGS.
22. ELECTRICAL SERVICE AND PANEL. - SEE ELECTRICAL DRAWINGS.

"PRELIMINARY NOT FOR CONSTRUCTION 85% PROGRESS SET"

MARKET HALLS ARCHITECTURAL CORP. NO. 1001 1/2

THE PROPOSED BUILDING FOR: LONEDELL BUS GARAGE LONEDELL, MISSOURI

CONSULTING ENGINEERS  
 Civil Engineer: [Name]  
 Structural Engineer: [Name]  
 Mechanical Engineer: [Name]  
 MEP Engineer: [Name]

HOENER ARCHITECTS, LLC  
 6707 PLANNING AVENUE  
 ST. LOUIS, MISSOURI 63109  
 OFFICE: 314.781.9855 HoenerArchitects.com

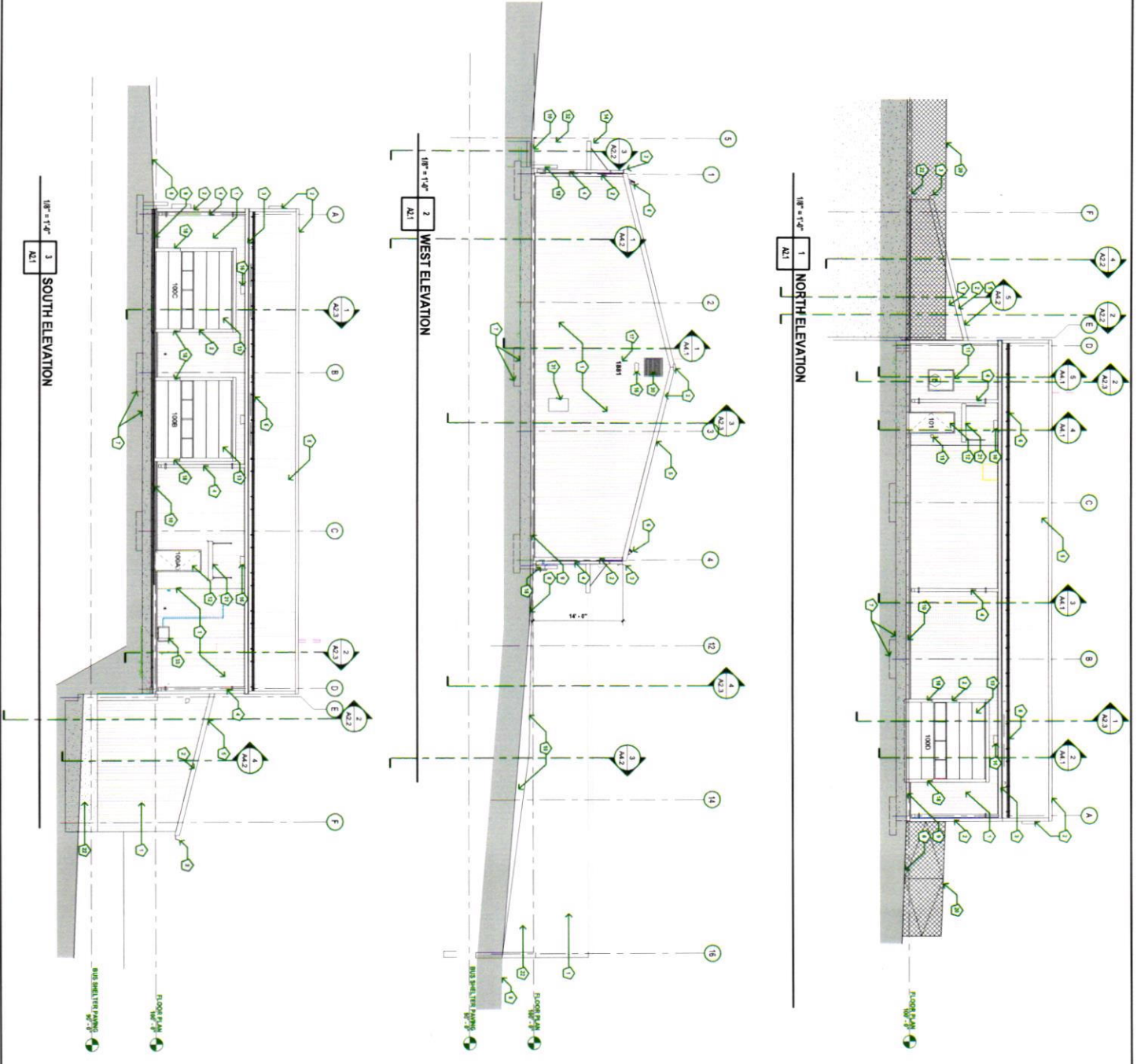
LONEDELL R-XIV SCHOOL DISTRICT

PROJ. NO. 25-43B  
 DATE ISSUED 06.04.2026  
 DATE REVISION

DRAWN BY BF  
 CHECKED BY MW  
 SHEET NO. A1.2

FLOOR PLAN





**BUILDING ELEVATION KEYED NOTES**

1. PREFINISHED METAL SIDING TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
2. PREFINISHED ROOF (LAWN / CORNER TRIM) TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
3. PREFINISHED GUTTERS WITH STOPS AT 4" O.C. TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER. SEE ROOF PLAN, WALL SECTIONS AND DA 17.5.
4. PREFINISHED DOWNROPTS WITH A BRACKET TOP AND BOTTOM TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER. SEE ROOF PLAN, WALL SECTIONS AND DA 17.5.
5. PREFINISHED ROOFING TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
6. ROW OF SHOWERS CONTINUOUS - SEE WALL SECTIONS AND SPECS.
7. DASHED LINE INDICATES APPROXIMATE STRUCTURAL CONCRETE FOUNDATION BOUNDARY.
8. GRADE VARIED - SEE CIVIL DRAWINGS FOR MORE INFORMATION.
9. EXPOSED CONCRETE FOUNDATION. SEE STRUCTURAL DRAWINGS.
10. CONCRETE EXTERIOR FINISH / SIDEWALK. SEE SITE PLAN FOR MORE INFORMATION.
11. ALUMINUM FIXED WINDOW UNIT W/ INSULATED GLAZING.
12. EXTERIOR INSULATED PER DOORS) AND ALUMINUM FRAME GLAZING. SEE DOOR SCHEDULE AND SPECS.
13. EXTERIOR PREFINISHED INSULATED OVERHEAD SECTIONAL DOOR WITH TRACKS AND STAYS.
14. COVERED STAIRWELL BELOW.
15. HOOK BOX - ANCHOR TO MAINTAIN PER MANUFACTURER'S RECOMMENDATIONS. COMPONENT LOCATION WITH THE MANUFACTURER.
16. EXTERIOR LIGHT FIXTURE. SEE ELECTRICAL DRAWINGS.
17. 8" TALL SIGN (BLACK) - ADDRESS. SEE SPECS. ANCHOR TO SIGNA.
18. EXTENSION PIPE ROLL-UP. SEE DA DETAIL 16.5F.
19. NEW 6" HIGH CHAINLINK FENCE BY THE SCHOOL DISTRICT. SEE SITE PLAN FOR MORE INFORMATION.
20. NEW 6" X 6" HIGH CHAINLINK SWING GATES BY THE SCHOOL DISTRICT.
21. PREFINISHED 6" X 6" WIRE ROOF SUPPORTED ALUMINUM CARRY-BLIND WITH BRILL SHOUT ON ONE SIDE (DOOR SIDE) BY PEMB SUPPLIER. SEE DA 16.5Z.
22. CONCRETE RETAINING WALL. SEE STRUCTURAL DRAWINGS.
23. STEEL PIPE HANDRAILS - PAINT. SEE DA 5.2I AND 5.2K.
24. STEEL COLUMN, PAINT, COLUMN AND STEEL STRUCTURE TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
25. PREFINISHED METAL LINER PANELS OVER INSIDE OF PEAB ROOF PURLIN.
26. STEEL BENT FRAME BY THE PRE-ENGINEERED METAL BUILDING MANUFACTURER.
27. 4" CONCRETE FLOOR SLAB OVER 15 ML. VAPOR BARRIER AND 4" GRANULAR FILL. SLOPE TO DRAIN TROUGH OVERHEAD DOOR OPENINGS. SEE STRUCTURAL DRAWINGS.
28. 4" CONCRETE FLOOR SLAB OVER 15 ML. VAPOR BARRIER AND 4" GRANULAR FILL. SEE STRUCTURAL DRAWINGS.
29. METAL EXHAUST FAN AND LOWER. SEE MECHANICAL DRAWINGS.
30. METAL EXHAUST FAN AND LOWER. SEE MECHANICAL DRAWINGS.
31. INSTALL ELECTRICAL SERVICE. SEE ELECTRICAL DRAWINGS.
32. HORIZONTAL AT STAIR NETWORK.
33. CONDENSING UNIT - SEE MECHANICAL DRAWINGS.

"PRELIMINARY"  
 CONSTRUCTION  
 SET  
 85% PROGRESS

MARK HALLMES  
 LICENSED ARCHITECT/ENGINEER  
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THE PROPOSED BUILDING FOR:  
**LONEDELL BUS GARAGE**  
 LONEDELL, MISSOURI

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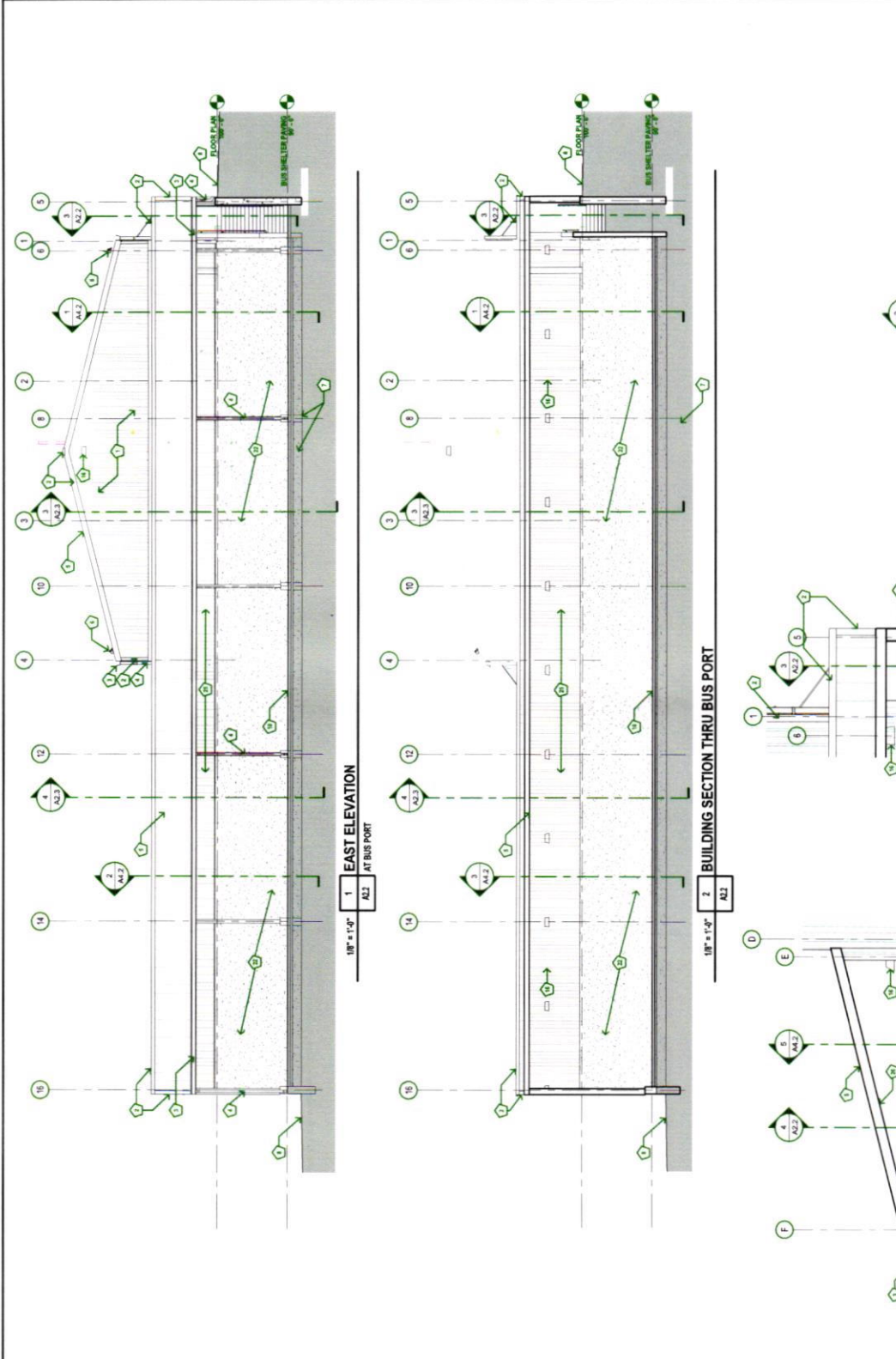
ALL DIMENSIONS MUST BE VERIFIED AT BUILDING REVISIONS. THIS DRAWING IS THE PROPOSED AND SHALL NOT BE COVERED OR CALCULATED WITHOUT THE ARCHITECT'S PERMISSION.

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FIELD VERIFY ALL DIMENSIONS WITHIN 1/8" TOLERANCE AND SPOT CHECK LOCATIONS ON WHICH DIMENSIONS ARE NOT SHOWN TO VERIFY CORRECT PROPORTIONS

- ### BUILDING ELEVATION KEYED NOTES
- 1 PREFINISHED METAL SIDING TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
  - 2 PREFINISHED RIDGE / LEAVE / CORNER TRIM TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
  - 3 PREFINISHED BUTTER WITH STRIPS AT 24" O.C. TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER. SEE ROOF PLAN, WALL SECTIONS AND DA 17.13.
  - 4 PREFINISHED DOWNSPOUTS WITH A BRACKET TOP AND BOTTOM TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER. SEE ROOF PLAN, WALL SECTIONS AND DA 17.14.
  - 5 PREFINISHED ROOFING TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
  - 6 ROW OF SNOWGUARDS CONTINUOUS - SEE WALL SECTIONS AND SPECS.
  - 7 DAMAGED LINE INDICATES APPROXIMATE STRUCTURAL CONCRETE FOUNDATION BELOW.
  - 8 GRADE VARIES - SEE CIVIL DRAWINGS FOR MORE INFORMATION.
  - 9 EXPOSED CONCRETE FOUNDATION. SEE STRUCTURAL DRAWINGS.
  - 10 CONCRETE EXTERIOR PAVING / SIDEWALK. SEE SITE PLAN FOR MORE INFORMATION.
  - 11 ALUMINUM FIXED WINDOW UNIT W/ 1" INSULATED GLAZING.
  - 12 EXTERIOR INSULATED RFP (DOORS) AND ALUMINUM FRAME (BLACK). SEE DOOR SCHEDULE AND DETAILS.
  - 13 EXTERIOR PREFINISHED INSULATED OVERHEAD SECTIONAL DOOR WITH TRACKS AND DOOR SEALS AROUND PERIMETER OF DOOR. SEE DOOR SCHEDULE AND DETAILS.
  - 14 COVERED STAIRWELL BEYOND.
  - 15 KNOX BOX - ANCHOR TO MASONRY PER MANUFACTURER RECOMMENDATIONS. COORDINATE LOCATION WITH FIRE MARSHAL.
  - 16 EXTERIOR LIGHT FIXTURE. SEE ELECTRICAL DRAWINGS.
  - 17 8" TALL SIGN (BLACK) - ADDRESS. SEE SPECS. ANCHOR TO SIGN.
  - 18 EXTERIOR PIPE BOLLARD. SEE DA DETAIL 16.91.
  - 19 NEW 6" X 6" HIGH CHAINLINK FENCE BY THE SCHOOL DISTRICT. SEE SITE PLAN FOR MORE INFORMATION.
  - 20 NEW 6'-0" HIGH CHAINLINK SWING GATES BY THE SCHOOL DISTRICT.
  - 21 PREFINISHED 6'-0" X 8'-0" WIDE RUD SUPPORTED ALUMINUM CANOPY (BLUR) WITH SPILL SPOUT ON ONE SIDE (DOOR SIDE) BY PEMB SUPPLIER. SEE DA 16.62.
  - 22 CONCRETE RETAINING WALL. SEE STRUCTURAL DRAWINGS.
  - 23 STEEL PIPE HANDRAILS - PAINT. SEE DA 6.21 AND 6.18.
  - 24 STEEL COLLUMS - PAINT. COLLUMS AND STEEL STRUCTURE TO BE BY THE PRE-ENGINEERED METAL BUILDING (PEMB) MANUFACTURER.
  - 25 PREFINISHED METAL LINER PANELS OVER INSIDE OF PEMB GUTTS.
  - 26 PREFINISHED METAL LINER PANELS OVER UNDERSIDE OF PEMB ROOF FURLINS.
  - 27 STEEL BENT FRAME BY THE PRE-ENGINEERED METAL BUILDING MANUFACTURER.
  - 28 6" CONCRETE FLOOR SLAB OVER 14 ML VAPOR BARRIER AND 4" GRANULAR FILL. SLOPE TO DRAIN THRU OVERHEAD DOOR OPENINGS. SEE STRUCTURAL DRAWINGS.
  - 29 4" CONCRETE FLOOR SLAB OVER 14 ML VAPOR BARRIER AND 4" GRANULAR FILL. SEE STRUCTURAL DRAWINGS.
  - 30 INSTALL EXHAUST FAN AND LOUVER. SEE MECHANICAL DRAWINGS.
  - 31 INSTALL ELECTRICAL SERVICE. SEE ELECTRICAL DRAWINGS.
  - 32 HANDRAIL AT STAIR BEYOND.
  - 33 CONDENSING UNIT - SEE MECHANICAL DRAWINGS.



1 EAST ELEVATION AT BUS PORT  
1/8" = 1'-0"  
A2.2

2 BUILDING SECTION THRU BUS PORT  
1/8" = 1'-0"  
A2.2

3 STAIR SECTION - LOOKING NORTH  
1/4" = 1'-0"  
A2.2

4 STAIR SECTION - LOOKING WEST  
1/4" = 1'-0"  
A2.2

5 EXTERIOR STAIR PLAN  
1/4" = 1'-0"  
A2.2

PRELIMINARY  
NOT FOR CONSTRUCTION  
85% PROGRESS  
SET

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THE PROPOSED BUS GARAGE  
LONDELL R-VOL SCHOOL DISTRICT

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ALL DIMENSIONS MUST BE FOR THE WORK AS EXECUTED. THE CONTRACTOR SHALL VERIFY THE FIELD CONDITIONS AND SHALL NOT BE CORRECTED UNLESS THE ARCHITECT AND ENGINEER CONSENT.

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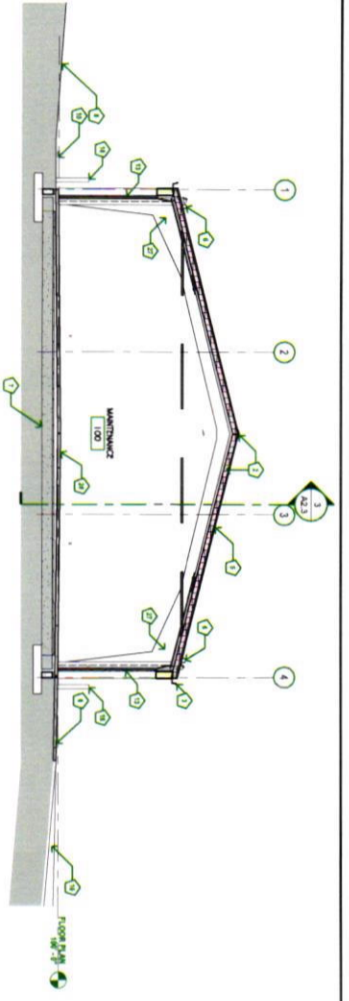
FIELD VERIFY ALL DIMENSIONS NOTED AS SHOWN. DIMENSIONS AND DETAILS ARE GOVERNED BY THE PROJECT SPECIFICATIONS.

25-43B THE PROPOSED BUILDING FOR LONDELL BUS GARAGE

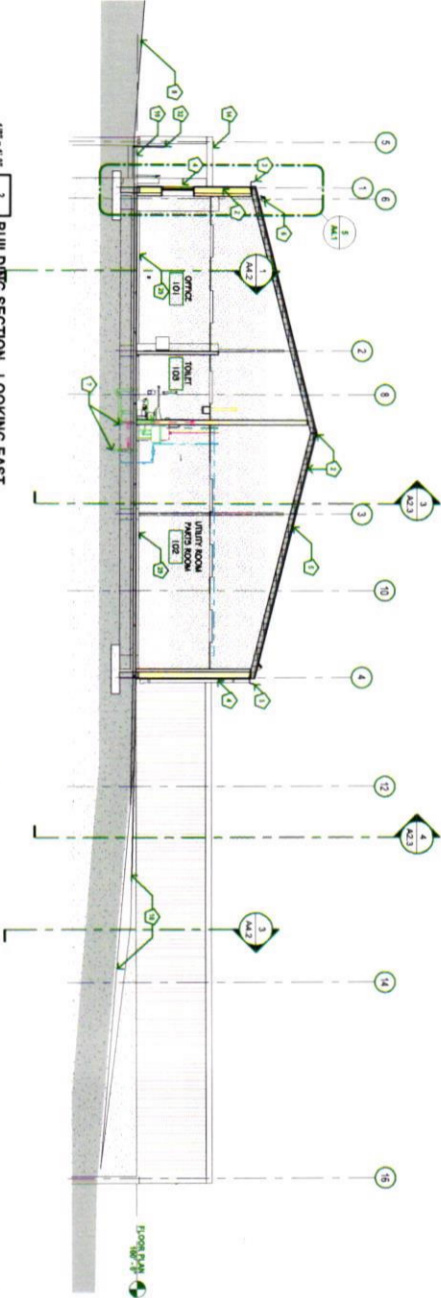
FIELD VERIFY ALL DIMENSIONS NOTED AS SHOWN. DIMENSIONS AND DETAILS ARE GOVERNED BY THE PROJECT SPECIFICATIONS.

25-43B THE PROPOSED BUILDING FOR LONDELL BUS GARAGE

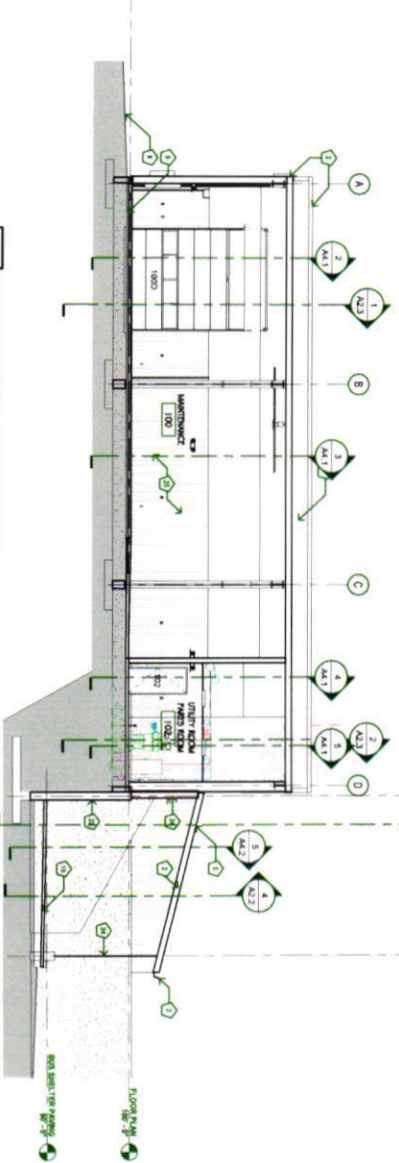
FIELD VERIFY ALL DIMENSIONS NOTED AS SHOWN. DIMENSIONS AND DETAILS ARE GOVERNED BY THE PROJECT SPECIFICATIONS.



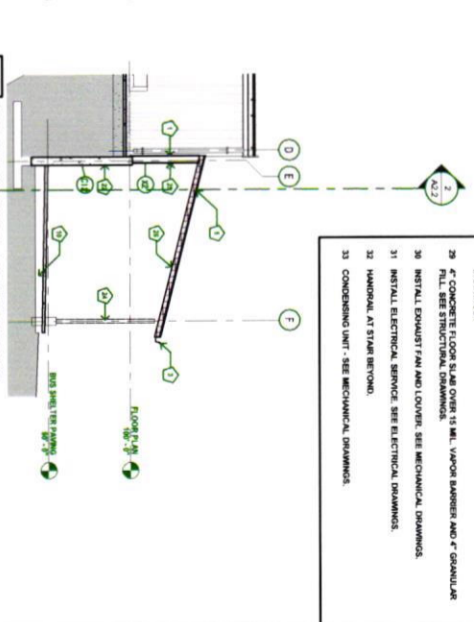
1'-0" = 1'-0"  
A2.3  
1 BUILDING SECTION - LOOKING EAST  
THROUGH GARAGE



1'-0" = 1'-0"  
A2.3  
2 BUILDING SECTION - LOOKING EAST  
THROUGH OFFICE STORAGE



1'-0" = 1'-0"  
A2.3  
3 BUILDING SECTION - LOOKING NORTH  
THROUGH MAINTENANCE



1'-0" = 1'-0"  
A2.3  
4 BUILDING SECTION - LOOKING NORTH  
THROUGH BUS FRONT ROOF

**BUILDING ELEVATION KEYED NOTES**

- 1 PREFINISHED METAL SIDING TO BE BY THE PREFINISHED METAL BUILDING (PFBM) MANUFACTURER.
- 2 PREFINISHED ROOF / RAFT / CORNER TRIM TO BE BY THE PREFINISHED METAL BUILDING (PFBM) MANUFACTURER.
- 3 PREFINISHED GUTTER WITH STAINLESS STEEL TO BE BY THE PREFINISHED METAL BUILDING (PFBM) MANUFACTURER. SEE ROOF PLAN, WALL SECTIONS AND DA 17/2.
- 4 PREFINISHED DOWNSPOUT WITH A BRACKET TOP AND BOTTOM TO BE BY THE PREFINISHED METAL BUILDING (PFBM) MANUFACTURER. SEE ROOF PLAN, WALL SECTIONS AND DA 17/3.
- 5 PREFINISHED ROOFING TO BE BY THE PREFINISHED METAL BUILDING (PFBM) MANUFACTURER.
- 6 ROW OF SHIMPOUSERS CONTINUOUS - SEE WALL SECTIONS AND SPECS.
- 7 DAMAGED LINE INDICATES APPROXIMATE STRUCTURAL CONCRETE FOUNDATION BELOW.
- 8 GRADE VARIES - SEE CIVIL DRAWINGS FOR MORE INFORMATION.
- 9 EXPOSED CONCRETE FOUNDATION. SEE STRUCTURAL DRAWINGS.
- 10 CONCRETE EXTERIOR FINISH / SERNALMA. SEE SITE PLAN FOR MORE INFORMATION.
- 11 ALUMINUM FINED WINDOW/LIGHT W/ 1" INSULATED GLAZING.
- 12 EXTERIOR INSULATED (PIR DOOR) AND ALUMINUM FRAME (BLACK). SEE DOOR SCHEDULE AND DETAILS.
- 13 EXTERIOR PREFINISHED INSULATED OVERHEAD SECTIONAL DOOR WITH TRACKS AND ROLLERS. SEE DOOR SCHEDULE AND DETAILS.
- 14 COVERED STAIRWELL NETWORK.
- 15 KNOX BOX - ANCHOR TO MASONRY PER MANUFACTURERS RECOMMENDATION. COORDINATE LOCATION WITH FIRE MARSHAL.
- 16 EXTERIOR LIGHT FIXTURE. SEE ELECTRICAL DRAWINGS.
- 17 4" TALL SIGN (BLACK). ADDRESS. SEE SPECS. ANCHOR TO SOING.
- 18 EXTERIOR PIPE ROLLER. SET DA DETAIL 15/41.
- 19 NEW 6" HIGH CHAIN-LINK FENCE BY THE SCHOOL. DISTRICT. SEE SITE PLAN FOR MORE INFORMATION.
- 20 NEW 6" HIGH CHAIN-LINK SWING GATES BY THE SCHOOL. DISTRICT.
- 21 PREFINISHED 6" x 6" x 4" PIPES FOR SUPPORTED ALUMINUM CANTILEVER RAILS WITH PULL-SHOOT ON ONE END (DOOR SIDE) BY PFBM SUPPLIER. SEE DA 15/22.
- 22 CONCRETE RETAINING WALL. SEE STRUCTURAL DRAWINGS.
- 23 STEEL PIPE HANDWALLS - PAINT. SEE DA 12/1 AND 12/6.
- 24 STEEL COLUMNS - PAINT. COILING AND STEEL STRUCTURE TO BE BY THE PREFINISHED METAL BUILDING (PFBM) MANUFACTURER.
- 25 PREFINISHED METAL LINER PANELS OVER INSIDE OF PFBM GATE.
- 26 PREFINISHED METAL LINER PANELS OVER INSIDERSIDE OF PFBM ROOF PERLINS.
- 27 STEEL BEAM FRAME BY THE PREFINISHED METAL BUILDING MANUFACTURER.
- 28 4" CONCRETE FLOOR SLAB OVER 15 MI. VAPOR BARRIER AND 4" GRANULAR FILL. SEE STRUCTURAL DRAWINGS.
- 29 4" CONCRETE FLOOR SLAB OVER 15 MI. VAPOR BARRIER AND 4" GRANULAR FILL. SEE STRUCTURAL DRAWINGS.
- 30 METAL EXHAUST / FAN AND COVER. SEE MECHANICAL DRAWINGS.
- 31 INSTALL ELECTRICAL SERVICE. SEE ELECTRICAL DRAWINGS.
- 32 HORIZONTAL AT STAIR NETWORK.
- 33 CONDENSING UNIT - SEE MECHANICAL DRAWINGS.

PRELIMINARY  
NOT FOR  
CONSTRUCTION  
50% PROGRESS  
SET

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THE PROPOSED BUILDING FOR:  
**LONEDELL BUS GARAGE**  
LONEDELL, MISSOURI

LONEDELL R-XIV SCHOOL DISTRICT  
**HOENER ARCHITECTS, LLC**  
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3/8" = 1'-0" 1 WALL SECTION  
ENDWALL AT GRADE

3/8" = 1'-0" 2 WALL SECTION  
AT OVERHEAD DOOR

3/8" = 1'-0" 3 WALL SECTION  
AT SEWER WALL

3/8" = 1'-0" 4 WALL SECTION  
AT MAIN DOOR

3/8" = 1'-0" 5 WALL SECTION  
AT EXTERIOR WINDOW

"PRELIMINARY"  
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CONSTRUCTION  
95% PROGRESS  
SET

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THE PROPOSED BUILDING FOR:  
**LONEDELL BUS GARAGE**  
LONEDELL R-XIV SCHOOL DISTRICT  
LONEDELL, MISSOURI

**HOENER ARCHITECTS, LLC**  
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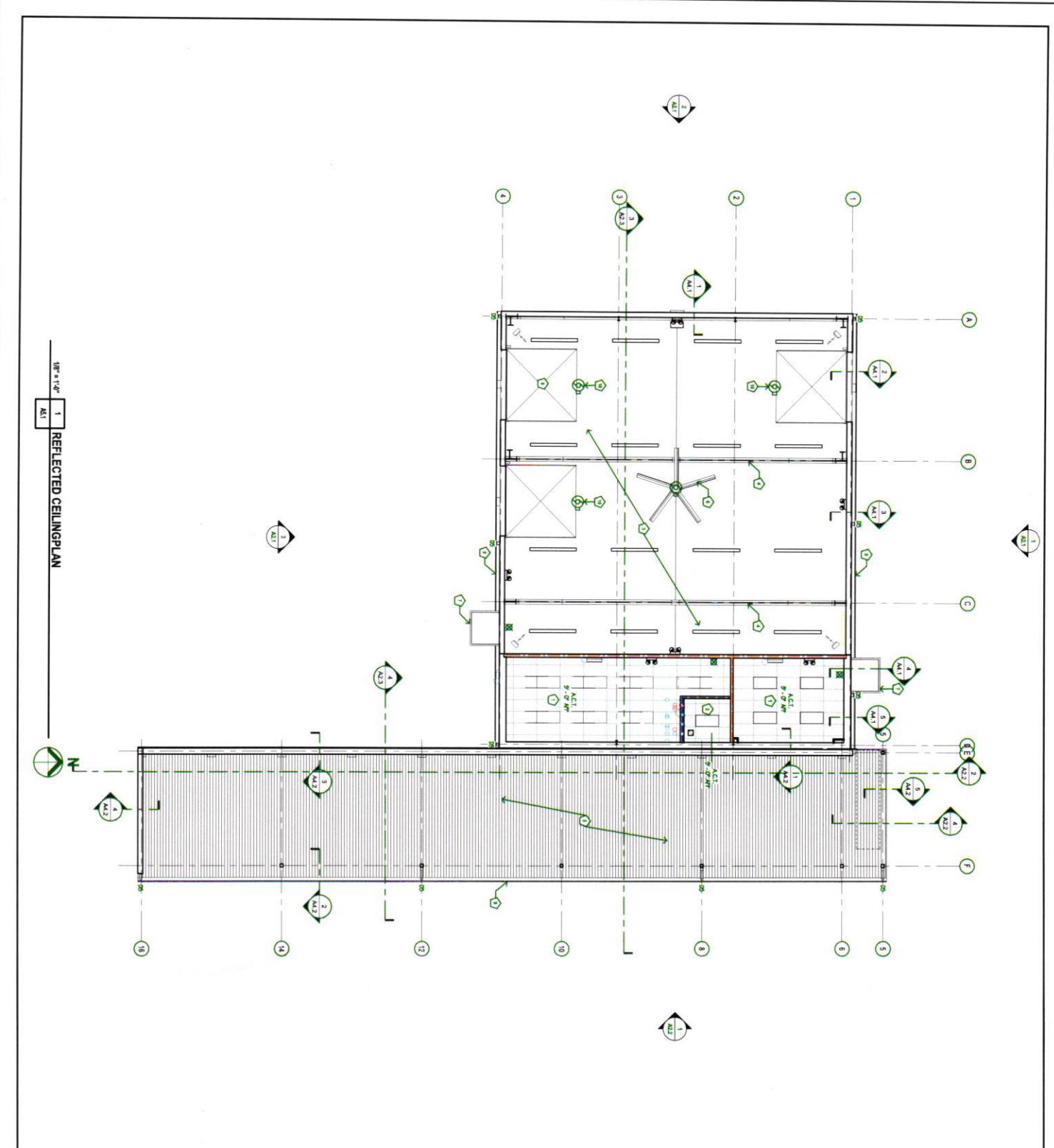
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WALL SECTIONS





1/8" = 1'-0" REFLECTED CEILING PLAN

**CEILING PLAN GENERAL NOTES**

1. SEE DA 1407 FOR GENERAL FINISHES OF CEILING GRID.
2. SEE DA 1411 FOR GENERAL FINISHES OF LIGHT FIXTURES.
3. SEE DA 1412 FOR GENERAL FINISHES OF LIGHT FIXTURES.
4. SEE DA 1413 FOR GENERAL FINISHES OF LIGHT FIXTURES.
5. SEE DA 1414 FOR GENERAL FINISHES OF LIGHT FIXTURES.
6. SEE DA 1415 FOR GENERAL FINISHES OF LIGHT FIXTURES.
7. SEE DA 1416 FOR GENERAL FINISHES OF LIGHT FIXTURES.
8. SEE DA 1417 FOR GENERAL FINISHES OF LIGHT FIXTURES.
9. SEE DA 1418 FOR GENERAL FINISHES OF LIGHT FIXTURES.
10. SEE DA 1419 FOR GENERAL FINISHES OF LIGHT FIXTURES.
11. SEE DA 1420 FOR GENERAL FINISHES OF LIGHT FIXTURES.
12. SEE DA 1421 FOR GENERAL FINISHES OF LIGHT FIXTURES.
13. SEE DA 1422 FOR GENERAL FINISHES OF LIGHT FIXTURES.
14. SEE DA 1423 FOR GENERAL FINISHES OF LIGHT FIXTURES.
15. SEE DA 1424 FOR GENERAL FINISHES OF LIGHT FIXTURES.
16. SEE DA 1425 FOR GENERAL FINISHES OF LIGHT FIXTURES.
17. SEE DA 1426 FOR GENERAL FINISHES OF LIGHT FIXTURES.
18. SEE DA 1427 FOR GENERAL FINISHES OF LIGHT FIXTURES.
19. SEE DA 1428 FOR GENERAL FINISHES OF LIGHT FIXTURES.
20. SEE DA 1429 FOR GENERAL FINISHES OF LIGHT FIXTURES.
21. SEE DA 1430 FOR GENERAL FINISHES OF LIGHT FIXTURES.

**CEILING PLAN LEGEND**

- LEGEND**
- ACROUSTICAL TILE CEILING IN SUSPENDED GRID. SEE DA 14.01C.
  - MOISTURE RESISTANT ACROUSTICAL TILE CEILING IN SUSPENDED GRID. SEE DA 14.01E.
  - EXPOSED FIBER INSULATION, ROOF PURLINS AND STRUCTURE.
  - STEEL BENT FRAME BY THE PREENGINEERED METAL BUILDING MANUFACTURER.
  - PREFINISHED METAL LINER PANELS OVER UNDERSIDE OF PURLINS.
  - LARGE PAN UNIT. SEE ELECTRICAL DRAWINGS.
  - PREFINISHED 5'-0" X 6'-0" WIND ROOF SUPPORTED ALUMINUM CANOPY (BLUE) WITH SPILL SPOUT ON ONE SIDE (DOOR SIDE) BY FIBER SUPPLIER. SEE DA 10.02.
  - PREFINISHED GUTTERS WITH STRAPS AT 2'-0" O.C. TO BE BY THE PREENGINEERED METAL BUILDING (PMB) MANUFACTURER. SEE ROOF PLAN WALL SECTIONS AND DA 17.04.
  - EXTERIOR PREFINISHED INSULATED OVERHEAD SECTIONAL DOOR WITH TRACKS AND DOOR SEALS AROUND PERIMETER OF DOOR. SEE DOOR SCHEDULE AND ELECTRICAL DRAWINGS AND SPEC.
  - INSTALL AUTORIZED OVERHEAD DOOR OPERATOR AS INDICATED. SEE ELECTRICAL DRAWINGS AND SPEC.

**REF. CEILING PLAN KEYED NOTES**

1. ACROUSTICAL TILE CEILING IN SUSPENDED GRID. SEE DA 14.01C.
2. MOISTURE RESISTANT ACROUSTICAL TILE CEILING IN SUSPENDED GRID. SEE DA 14.01E.
3. EXPOSED FIBER INSULATION, ROOF PURLINS AND STRUCTURE.
4. STEEL BENT FRAME BY THE PREENGINEERED METAL BUILDING MANUFACTURER.
5. PREFINISHED METAL LINER PANELS OVER UNDERSIDE OF PURLINS.
6. LARGE PAN UNIT. SEE ELECTRICAL DRAWINGS.
7. PREFINISHED 5'-0" X 6'-0" WIND ROOF SUPPORTED ALUMINUM CANOPY (BLUE) WITH SPILL SPOUT ON ONE SIDE (DOOR SIDE) BY FIBER SUPPLIER. SEE DA 10.02.
8. PREFINISHED GUTTERS WITH STRAPS AT 2'-0" O.C. TO BE BY THE PREENGINEERED METAL BUILDING (PMB) MANUFACTURER. SEE ROOF PLAN WALL SECTIONS AND DA 17.04.
9. EXTERIOR PREFINISHED INSULATED OVERHEAD SECTIONAL DOOR WITH TRACKS AND DOOR SEALS AROUND PERIMETER OF DOOR. SEE DOOR SCHEDULE AND ELECTRICAL DRAWINGS AND SPEC.
10. INSTALL AUTORIZED OVERHEAD DOOR OPERATOR AS INDICATED. SEE ELECTRICAL DRAWINGS AND SPEC.

"PRELIMINARY"  
CONSTRUCTION  
NOT FOR  
SET

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THE PROPOSED BUILDING FOR:  
**LONEDELL BUS GARAGE**  
LONEDELL, MISSOURI  
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OFFICE: 314. 781.9855 HoenerArchitects.com

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**SPECIAL INSPECTION TABLES**

TABLE NO.	DESCRIPTION	INSPECTION TYPE	INSPECTION FREQUENCY	INSPECTION POINTS
1	STEEL CONNECTIONS TO BE WELDED	WELDED	AS REQUIRED	AS REQUIRED
2	STEEL CONNECTIONS TO BE BOLTED	BOLTED	AS REQUIRED	AS REQUIRED
3	STEEL CONNECTIONS TO BE BOLTED AND WELDED	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
4	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
5	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR AND THE INSPECTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED

TABLE NO.	DESCRIPTION	INSPECTION TYPE	INSPECTION FREQUENCY	INSPECTION POINTS
1	STEEL CONNECTIONS TO BE WELDED	WELDED	AS REQUIRED	AS REQUIRED
2	STEEL CONNECTIONS TO BE BOLTED	BOLTED	AS REQUIRED	AS REQUIRED
3	STEEL CONNECTIONS TO BE BOLTED AND WELDED	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
4	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
5	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR AND THE INSPECTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED

TABLE NO.	DESCRIPTION	INSPECTION TYPE	INSPECTION FREQUENCY	INSPECTION POINTS
1	STEEL CONNECTIONS TO BE WELDED	WELDED	AS REQUIRED	AS REQUIRED
2	STEEL CONNECTIONS TO BE BOLTED	BOLTED	AS REQUIRED	AS REQUIRED
3	STEEL CONNECTIONS TO BE BOLTED AND WELDED	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
4	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
5	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR AND THE INSPECTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED

TABLE NO.	DESCRIPTION	INSPECTION TYPE	INSPECTION FREQUENCY	INSPECTION POINTS
1	STEEL CONNECTIONS TO BE WELDED	WELDED	AS REQUIRED	AS REQUIRED
2	STEEL CONNECTIONS TO BE BOLTED	BOLTED	AS REQUIRED	AS REQUIRED
3	STEEL CONNECTIONS TO BE BOLTED AND WELDED	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
4	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
5	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR AND THE INSPECTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED

TABLE NO.	DESCRIPTION	INSPECTION TYPE	INSPECTION FREQUENCY	INSPECTION POINTS
1	STEEL CONNECTIONS TO BE WELDED	WELDED	AS REQUIRED	AS REQUIRED
2	STEEL CONNECTIONS TO BE BOLTED	BOLTED	AS REQUIRED	AS REQUIRED
3	STEEL CONNECTIONS TO BE BOLTED AND WELDED	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
4	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED
5	STEEL CONNECTIONS TO BE BOLTED AND WELDED TO BE INSPECTED BY THE CONTRACTOR AND THE INSPECTOR	BOLTED AND WELDED	AS REQUIRED	AS REQUIRED

**LIST OF ABBREVIATIONS**

AL	ALUMINUM	AS	AS REQUIRED
ALT	ALTERNATE	AS	AS REQUIRED
AM	ALUMINUM	AS	AS REQUIRED
APP	APPLICATION	AS	AS REQUIRED
APPROX	APPROXIMATE	AS	AS REQUIRED
AT	AT THE	AS	AS REQUIRED
ATB	AT THE BOTTOM	AS	AS REQUIRED
ATC	AT THE TOP	AS	AS REQUIRED
ATD	AT THE END	AS	AS REQUIRED
ATL	AT THE LEFT	AS	AS REQUIRED
ATR	AT THE RIGHT	AS	AS REQUIRED
ATV	AT THE VERTICAL	AS	AS REQUIRED
ATW	AT THE WIDTH	AS	AS REQUIRED
ATX	AT THE EXTENSION	AS	AS REQUIRED
ATY	AT THE YIELD	AS	AS REQUIRED
ATZ	AT THE ZONE	AS	AS REQUIRED
AV	AVAILABILITY	AS	AS REQUIRED
AVG	AVERAGE	AS	AS REQUIRED
AW	AIRWAY	AS	AS REQUIRED
AWD	AIRWAY WIDTH	AS	AS REQUIRED
AWL	AIRWAY LENGTH	AS	AS REQUIRED
AWM	AIRWAY WIDTH AND LENGTH	AS	AS REQUIRED
AWN	AIRWAY WIDTH AND NATURE	AS	AS REQUIRED
AWO	AIRWAY WIDTH AND ORIENTATION	AS	AS REQUIRED
AWP	AIRWAY WIDTH AND POSITION	AS	AS REQUIRED
AWX	AIRWAY WIDTH AND EXTENSION	AS	AS REQUIRED
AWY	AIRWAY WIDTH AND YIELD	AS	AS REQUIRED
AWZ	AIRWAY WIDTH AND ZONE	AS	AS REQUIRED
AX	AXIS	AS	AS REQUIRED
AXC	AXIS CENTER	AS	AS REQUIRED
AXD	AXIS CENTER DISTANCE	AS	AS REQUIRED
AXE	AXIS CENTER EXTENSION	AS	AS REQUIRED
AXF	AXIS CENTER FINISH	AS	AS REQUIRED
AXG	AXIS CENTER GROUND	AS	AS REQUIRED
AXH	AXIS CENTER HEIGHT	AS	AS REQUIRED
AXI	AXIS CENTER IDENTIFICATION	AS	AS REQUIRED
AXJ	AXIS CENTER JUNCTION	AS	AS REQUIRED
AXK	AXIS CENTER KIND	AS	AS REQUIRED
AXL	AXIS CENTER LOCATION	AS	AS REQUIRED
AXM	AXIS CENTER MATERIAL	AS	AS REQUIRED
AXN	AXIS CENTER NAME	AS	AS REQUIRED
AXO	AXIS CENTER NUMBER	AS	AS REQUIRED
AXP	AXIS CENTER POSITION	AS	AS REQUIRED
AXQ	AXIS CENTER QUANTITY	AS	AS REQUIRED
AXR	AXIS CENTER RADIUS	AS	AS REQUIRED
AXS	AXIS CENTER SHAPE	AS	AS REQUIRED
AXT	AXIS CENTER SIZE	AS	AS REQUIRED
AXU	AXIS CENTER SOURCE	AS	AS REQUIRED
AXV	AXIS CENTER STATUS	AS	AS REQUIRED
AXW	AXIS CENTER TYPE	AS	AS REQUIRED
AXX	AXIS CENTER USE	AS	AS REQUIRED
AXY	AXIS CENTER VALUE	AS	AS REQUIRED
AXZ	AXIS CENTER WEIGHT	AS	AS REQUIRED
AY	AXIS CENTER YIELD	AS	AS REQUIRED
AYC	AXIS CENTER YIELD CENTER	AS	AS REQUIRED
AYD	AXIS CENTER YIELD DISTANCE	AS	AS REQUIRED
AYE	AXIS CENTER YIELD EXTENSION	AS	AS REQUIRED
AYF	AXIS CENTER YIELD FINISH	AS	AS REQUIRED
AYG	AXIS CENTER YIELD GROUND	AS	AS REQUIRED
AYH	AXIS CENTER YIELD HEIGHT	AS	AS REQUIRED
AYI	AXIS CENTER YIELD IDENTIFICATION	AS	AS REQUIRED
AYJ	AXIS CENTER YIELD JUNCTION	AS	AS REQUIRED
AYK	AXIS CENTER YIELD KIND	AS	AS REQUIRED
AYL	AXIS CENTER YIELD LOCATION	AS	AS REQUIRED
AYM	AXIS CENTER YIELD MATERIAL	AS	AS REQUIRED
AYN	AXIS CENTER YIELD NAME	AS	AS REQUIRED
AYO	AXIS CENTER YIELD NUMBER	AS	AS REQUIRED
AYP	AXIS CENTER YIELD POSITION	AS	AS REQUIRED
AYQ	AXIS CENTER YIELD QUANTITY	AS	AS REQUIRED
AYR	AXIS CENTER YIELD RADIUS	AS	AS REQUIRED
AYS	AXIS CENTER YIELD SHAPE	AS	AS REQUIRED
AYT	AXIS CENTER YIELD SIZE	AS	AS REQUIRED
AYU	AXIS CENTER YIELD SOURCE	AS	AS REQUIRED
AYV	AXIS CENTER YIELD STATUS	AS	AS REQUIRED
AYW	AXIS CENTER YIELD TYPE	AS	AS REQUIRED
AYX	AXIS CENTER YIELD USE	AS	AS REQUIRED
AYY	AXIS CENTER YIELD VALUE	AS	AS REQUIRED
AYZ	AXIS CENTER YIELD WEIGHT	AS	AS REQUIRED
AZ	AXIS CENTER ZONE	AS	AS REQUIRED
AZC	AXIS CENTER ZONE CENTER	AS	AS REQUIRED
AZD	AXIS CENTER ZONE DISTANCE	AS	AS REQUIRED
AZE	AXIS CENTER ZONE EXTENSION	AS	AS REQUIRED
AZF	AXIS CENTER ZONE FINISH	AS	AS REQUIRED
AZG	AXIS CENTER ZONE GROUND	AS	AS REQUIRED
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AZI	AXIS CENTER ZONE IDENTIFICATION	AS	AS REQUIRED
AZJ	AXIS CENTER ZONE JUNCTION	AS	AS REQUIRED
AZK	AXIS CENTER ZONE KIND	AS	AS REQUIRED
AZL	AXIS CENTER ZONE LOCATION	AS	AS REQUIRED
AZM	AXIS CENTER ZONE MATERIAL	AS	AS REQUIRED
AZN	AXIS CENTER ZONE NAME	AS	AS REQUIRED
AZO	AXIS CENTER ZONE NUMBER	AS	AS REQUIRED
AZP	AXIS CENTER ZONE POSITION	AS	AS REQUIRED
AZQ	AXIS CENTER ZONE QUANTITY	AS	AS REQUIRED
AZR	AXIS CENTER ZONE RADIUS	AS	AS REQUIRED
AZS	AXIS CENTER ZONE SHAPE	AS	AS REQUIRED
AZT	AXIS CENTER ZONE SIZE	AS	AS REQUIRED
AZU	AXIS CENTER ZONE SOURCE	AS	AS REQUIRED
AZV	AXIS CENTER ZONE STATUS	AS	AS REQUIRED
AZW	AXIS CENTER ZONE TYPE	AS	AS REQUIRED
AZX	AXIS CENTER ZONE USE	AS	AS REQUIRED
AZY	AXIS CENTER ZONE VALUE	AS	AS REQUIRED
AZZ	AXIS CENTER ZONE WEIGHT	AS	AS REQUIRED

**LONEDELL BUS GARAGE**

LONEDELL R-XIV SCHOOL DISTRICT      LONEDELL, MISSOURI

**HOENER ARCHITECTS, LLC**      CONSULTING ENGINEERS

6707 PLAINVIEW AVENUE      Civil Engineer      1000 LOCKN HIGWAY      MISSOURI ENGINEERS

ST. LOUIS, MISSOURI 63109      Structural Engineer      FAIRMONT HEIGHTS, IL 6208      314.648.2322

OFFICE: 314.781.9855      HoenerArchitects.com      1414 E. 68th St.      314.648.2322

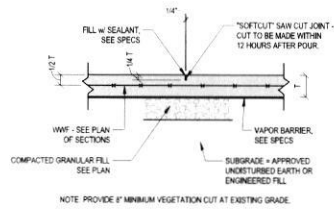
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**S1.2**

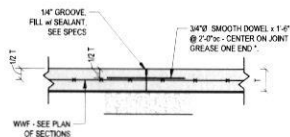
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DATE ISSUED: PRL NO. 25-43B  
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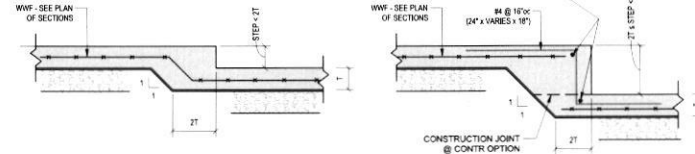
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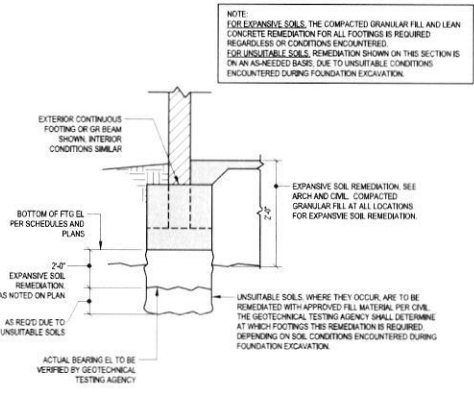
**1** TYPICAL SLAB ON GRADE CONTROL JOINT DETAIL  
NTS



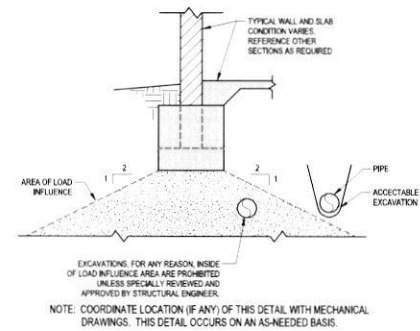
**2** TYPICAL SLAB ON GRADE CONSTRUCTION JOINT DETAIL  
NTS



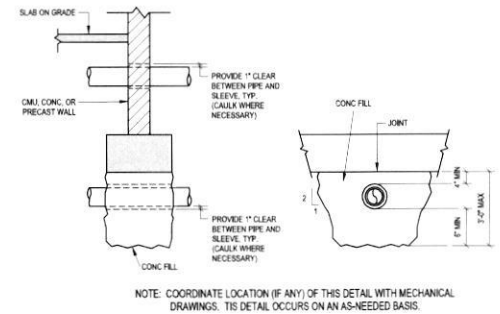
**3** TYPICAL SLAB ON GRADE STEP/DEPRESSION DETAIL  
NTS



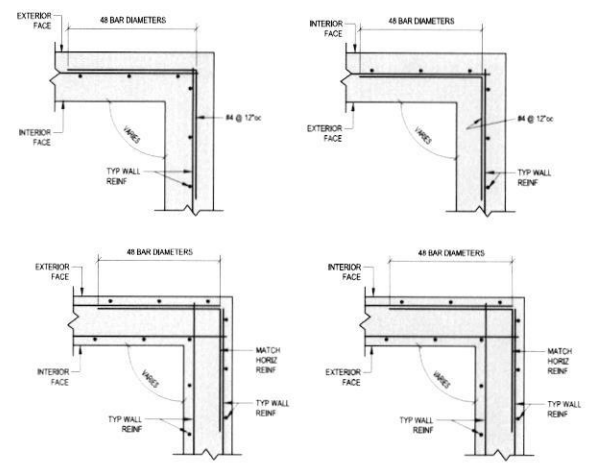
**4** TYPICAL EXPANSIVE AND/OR UNSUITABLE SOIL REMEDIATION  
NTS



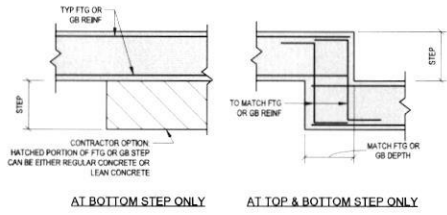
**5** PERMITTED EXCAVATIONS AT LOAD BEARING FOUNDATION  
NTS



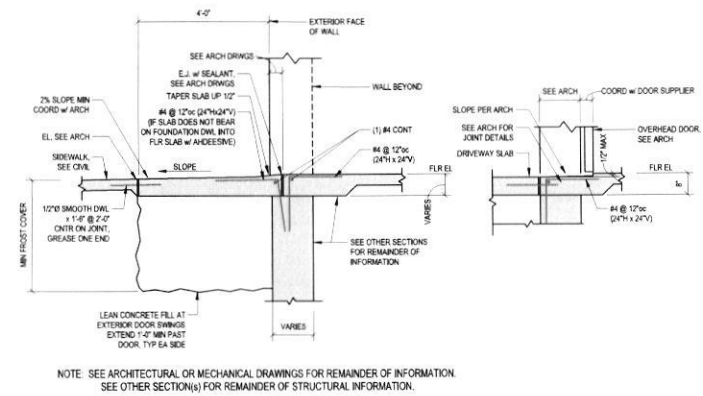
**6** TYPICAL PIPE THRU OR BELOW FOUNDATION  
NTS



**7** TYPICAL CONCRETE WALL CORNER REINFORCING  
NTS



**8** TYPICAL FOOTING OR GRADE BEAM STEP  
NTS



**9** TYPICAL FROST BLOCK AT ALL EXTERIOR DOOR(S), U.N.O.  
NTS

NOTE: FOR EXPANSIVE SOILS, THE COMPACTED GRANULAR FILL AND LEAN CONCRETE REMEDIATION FOR ALL FOOTINGS IS REQUIRED REGARDLESS OF CONDITIONS ENCOUNTERED. FOR UNSUITABLE SOILS, REMEDIATION SHOWN ON THIS SECTION IS ON AN AS-NEEDED BASIS, DUE TO UNSUITABLE CONDITIONS ENCOUNTERED DURING FOUNDATION EXCAVATION.

NOTE: COORDINATE LOCATION (IF ANY) OF THIS DETAIL WITH MECHANICAL DRAWINGS. THIS DETAIL OCCURS ON AN AS-NEEDED BASIS.

NOTE: COORDINATE LOCATION (IF ANY) OF THIS DETAIL WITH MECHANICAL DRAWINGS. THIS DETAIL OCCURS ON AN AS-NEEDED BASIS.

PRELIMINARY  
NOT FOR  
CONSTRUCTION

NATHAN KREKLE  
LICENSE #2000022124  
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NO. 2003024453

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Issue Date

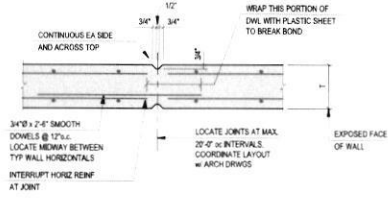
DATE REVISION

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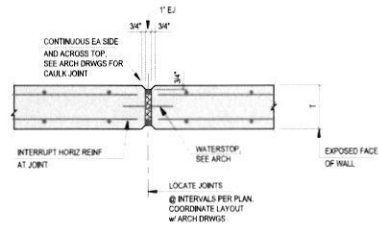
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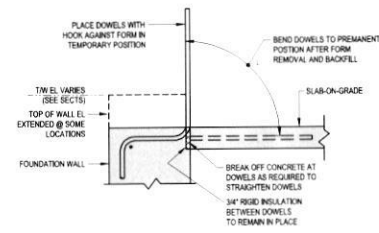
S1.3



1 TYPICAL CONTROL JOINT  
NTS



2 TYPICAL EXPANSION JOINT  
NTS



3 TYPICAL SLAB-ON-GRADE  
EDGE AT PERIMETER  
FOUNDATION WALL  
NTS

PRELIMINARY  
NOT FOR  
CONSTRUCTION

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LONEDELL BUS GARAGE  
LONEDELL R-XIV SCHOOL DISTRICT

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Issue Date  
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S1.4

26010.01

25-43B LONEDELL BUS GARAGE

TYPICAL DETAILS





S4.1

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DATE ISSUED

PROJ. NO. 25-43B

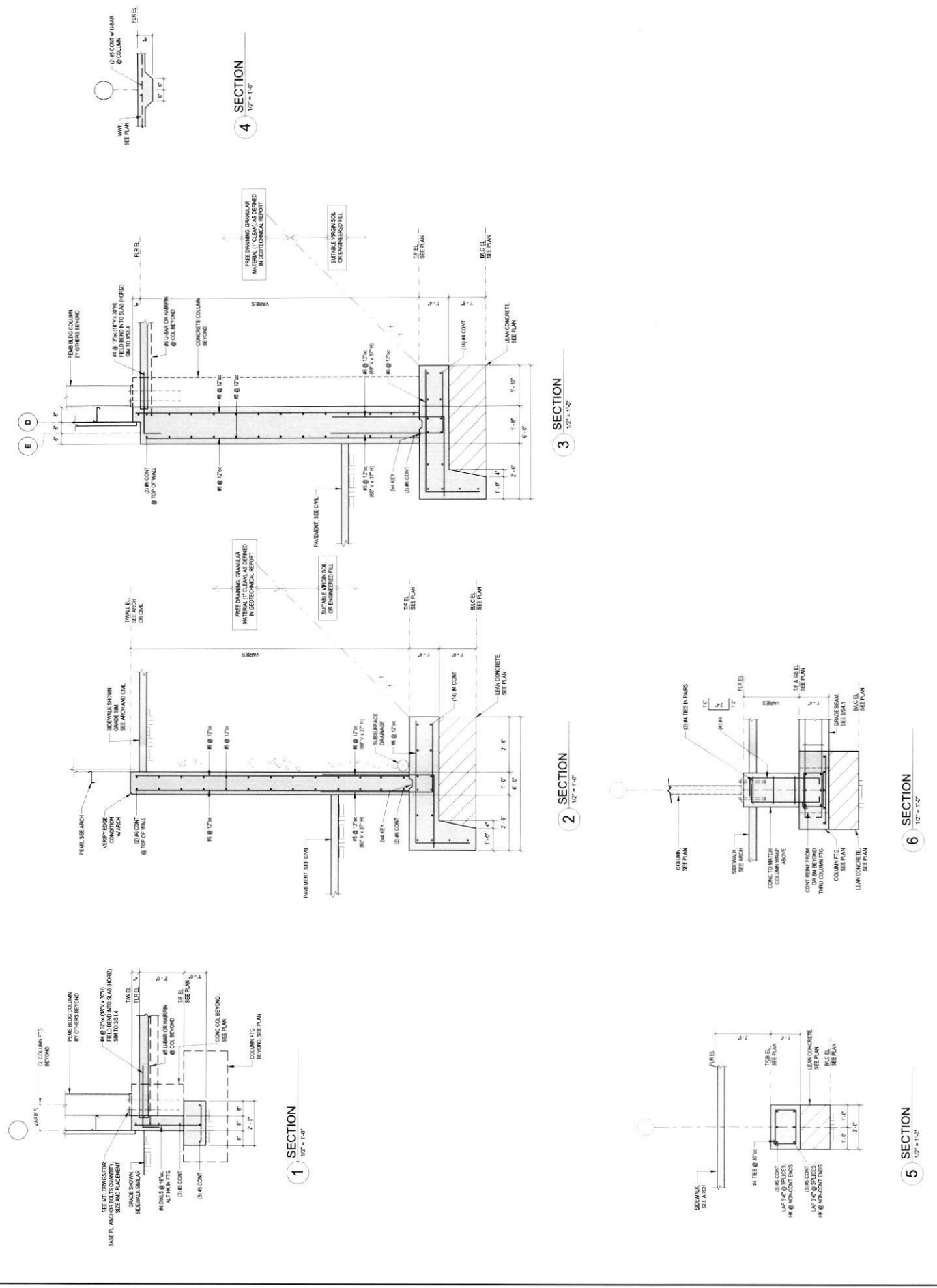
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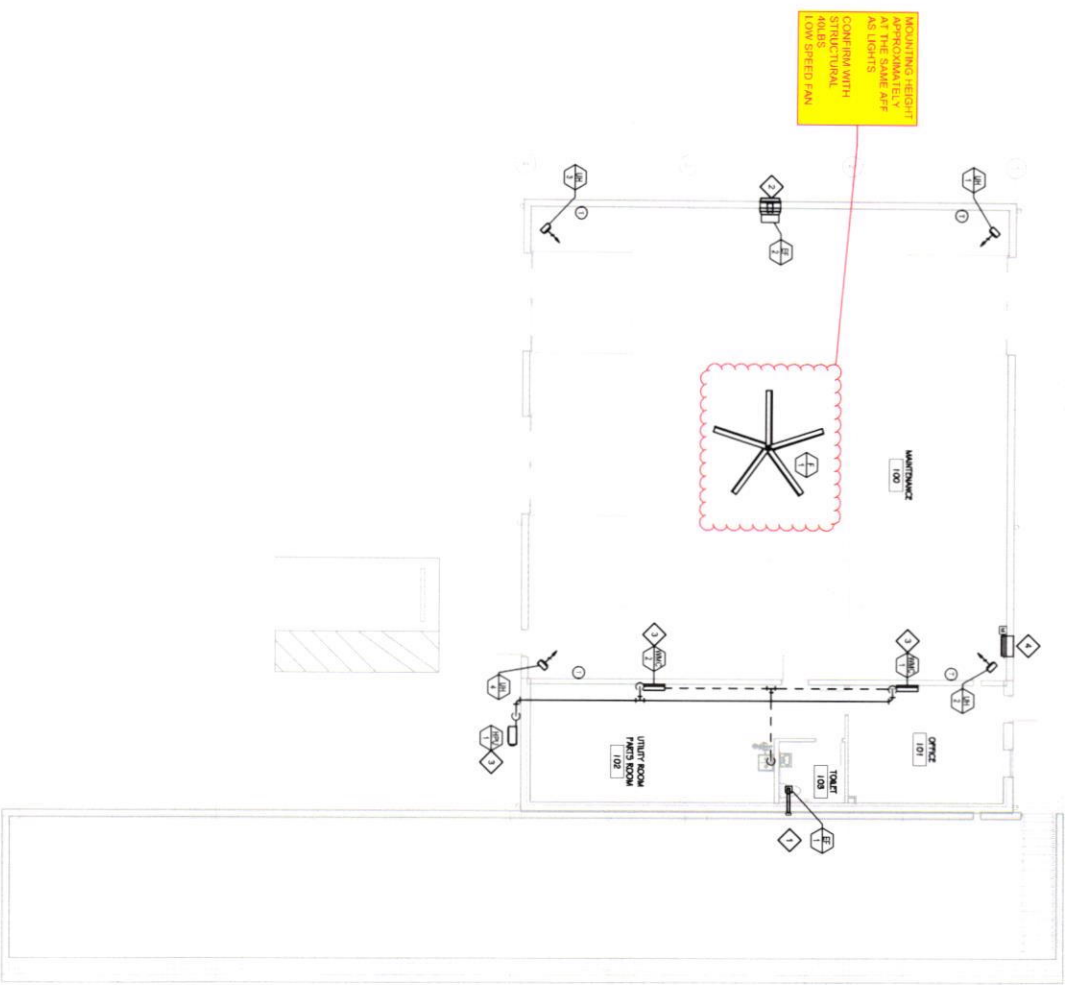


HOENER ARCHITECTS, LLC  
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LONDELL R-XYV SCHOOL DISTRICT  
 CONSULTING ENGINEERS  
 A2051 LLC  
 1508 NORTH HENRIKAY  
 ST. LOUIS, MO 63122  
 LICENSE #200202124  
 ENGINEERING CORP.  
 NO. 2003024463

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 CONSTRUCTION





1/8" = 1'-0" 1 FLOOR PLAN - MECHANICAL



**KEYED NOTES:**

- 1 PROVIDE CEILING FAN WITH RELATED GRALE. CONNECT # 8 DUCT TO FAN AND ROUTE OUT THE DUCT SHALL BE SHOWN WITH VENT CAP.
- 2 PROVIDE EXHAUST FAN WITH 1" AIR GAP. PROVIDE DAMPER AND LEAKAGE. LEAKAGE SHALL BE ESTIMATED WITH BROAD SCREEN. REPAIR MODEL LISTED ON EQUAL.
- 3 PROVIDE EXHAUST FAN WITH 1" AIR GAP. PROVIDE DAMPER AND LEAKAGE. LEAKAGE SHALL BE ESTIMATED WITH BROAD SCREEN. REPAIR MODEL LISTED ON EQUAL.
- 4 PROVIDE EXHAUST FAN WITH 1" AIR GAP. PROVIDE DAMPER AND LEAKAGE. LEAKAGE SHALL BE ESTIMATED WITH BROAD SCREEN. REPAIR MODEL LISTED ON EQUAL.

**SEQUENCE OF CONTROL:**

- EXHAUST FAN EF-1 SHALL BE ON/OFF VIA WALL SWITCH.
- EXHAUST FAN EF-2 SHALL BE ON/OFF VIA WALL SWITCH. WHEN FAN IS TURNED ON CONTROL DAMPER D1 WILL OPEN WHEN EF-1 IS OFF. CONTROL DAMPER D1 SHALL CLOSE DAMPER D1 WILL CLOSE ON POWER OUTAGE.
- WMC-1 AND WMC-2 SHALL BE CONTROLLED VIA WALL MOUNTED THERMOSTATS.
- WMC-1 SHALL BE THE SPLITER CONTROLLER.
- FAN F-1 SHALL BE ON/OFF VIA WALL SWITCH.
- UNIT HEATERS WILL BE CONTROLLED VIA WALL MOUNTED THERMOSTATS.

**FAN (EF, F) SCHEDULE**

MARK	MANUFACTURER MODEL NO.	CFM	FAN RPM	ESP W/L	VOLTHP	APPROX WEIGHT
EF-1	GREENHECK BR-448	75	908	0.29	2 MCA	12
EF-2	GREENHECK BR-448	200	1427	0.25	1.5 MCA	8
F-1	GREENHECK KC-4-140	1400	134	1.38	1.8 MCA	34

**AC SYSTEM (WMC-HPU) SCHEDULE**

MARK	MANUFACTURER MODEL NO.	CFM	MARK	MANUFACTURER MODEL NO.	VOLTHP
WMC-1	DAHNE PFM14149	18	HPU-1	DAHNE QDTH14149	200 24.3 MCA

**UNIT HEATER (UH) SCHEDULE**

MARK	MANUFACTURER MODEL NO.	TOTAL CAPACITY (BTU)	TEMP. RISE	SEIGNY AM (1/2")	POWER
UH-1	WITTENBERGER	9	97°	55	2.0HP

"PRELIMINARY NOT FOR CONSTRUCTION 95% PROGRESS SET"

CARTER SCHAEFER  
LICENSED PROFESSIONAL ENGINEERING CORP.  
NO. E-201196242-81

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MEP Engineer  
ASDC, LLC  
100 LINCOLN HIGHWAY  
FAIRVIEW HEIGHTS, E. 62208  
T. 618.628.1887

CONSULTING ENGINEERS  
Structural Engineer  
ABCO, LLC  
100 LINCOLN HIGHWAY  
FAIRVIEW HEIGHTS, E. 62208  
T. 618.628.1887

Civil Engineer  
COCHRAN CIVIL ENGINEERING  
2344 E. REFERENCE DR.  
UNION, MO 63084  
F. 618.334.6164

THE PROPOSED BUILDING FOR:  
**LONEDELL BUS GARAGE**  
LONEDELL, MISSOURI

LONEDELL R-XIV SCHOOL DISTRICT

**HOENER ARCHITECTS, LLC**  
6707 PLANVIEW AVENUE  
ST. LOUIS, MISSOURI 63109  
OFFICE: 314.781.9855 HoenerArchitects.com

ALL DIMENSIONS UNLESS OTHERWISE NOTED SHALL BE IN FEET AND INCHES. DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE ACCURACY OF THE ARCHITECT'S DIMENSIONS AND CALCULATED WEIGHTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.

PROJ. NO. 25-438  
DATE ISSUED 05.28.2026  
DATE REVISION

DRAWN BY DLH  
CHECKED BY MW  
SHEET NO. M2.01

25-438 THE PROPOSED BUILDING FOR: LONEDELL BUS GARAGE

PRELIMINARY  
NOT FOR CONSTRUCTION  
95% DESIGN

OWNER: LONDELL R-XIV SCHOOL DISTRICT  
PROJECT: LONDELL BUS GARAGE  
DATE: 05/28/2026

HOENER ARCHITECTS, LLC  
6707 PLAINVIEW AVENUE  
ST. LOUIS, MISSOURI 63109  
OFFICE: 314.781.9655  
hoenerarchitects.com

CONSULTING ENGINEERS  
LONDELL, MISSOURI

MECHANICAL ENGINEER  
NO. 000000000  
PLUMBING ENGINEER  
NO. 000000000  
ELECTRICAL ENGINEER  
NO. 000000000

MECHANICAL ENGINEER  
NO. 000000000  
PLUMBING ENGINEER  
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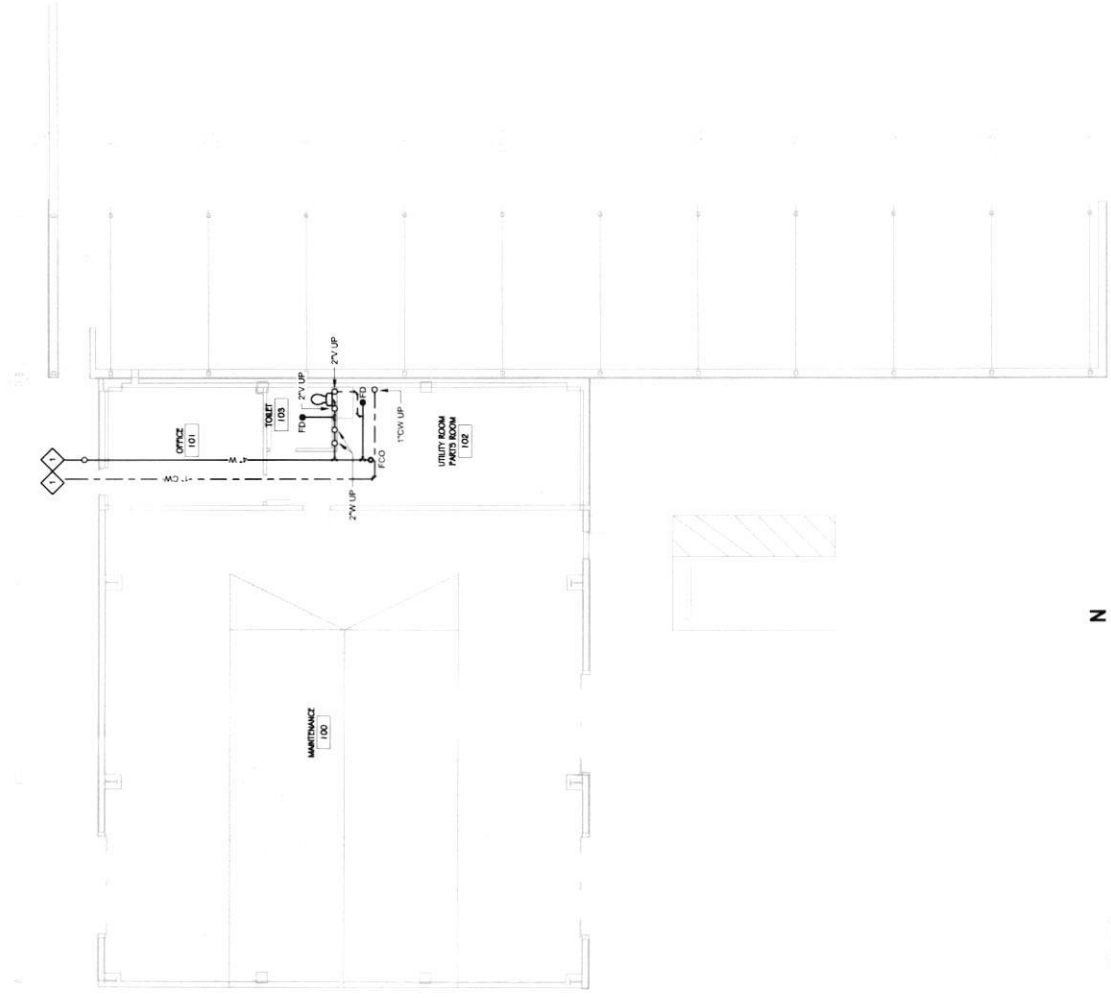
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NO. 000000000  
ELECTRICAL ENGINEER  
NO. 000000000

**GENERAL NOTES:**

- A. THESE PLANS ARE DIAGRAMMATIC IN NATURE AND REFLECT THE AVAILABLE INFORMATION OBTAINED FROM EXISTING RECORDS. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND BE RESPONSIBLE FOR VERIFYING EXACT LOCATIONS AND DIMENSIONS OF PIPING, EQUIPMENT, & COMPONENTS. VERIFY ALL DIMENSIONS AND LOCATIONS OF EXISTING SYSTEMS AND SHOWN ON THESE PLANS. VERIFY ALL DIMENSIONS AND LOCATIONS OF EXISTING SYSTEMS AND SHOWN ON THESE PLANS WITHOUT THEIR CONSENT.
- B. UNLESS NOTED, EXISTING SHOWN IN FADE-AWAY PEN, NEW, RELOCATED, OR DEMOLITION ITEMS SHOWN IN DARK HEAVY PEN.
- C. ALL WORK THAT IMPACTS BUILDING OPERATIONS TO BE SCHEDULED WITH OWNER.
- D. ALL WORK THAT IMPACTS BUILDING OPERATIONS TO BE SCHEDULED WITH OWNER.
- E. WASTE PIPING SHOWN ON THESE FLOOR PLANS OCCURS ABOVE CEILING UNLESS OTHERWISE NOTED.

**KEYED NOTES:**

SEE CIVIL FOR CONTINUATION.



1 UNDER FLOOR PLAN - PLUMBING  
1/8" = 1'-0"  
P2.01

1 P2.01

PROJ. NO. 25-43B  
DATE ISSUED 05.28.2026  
DATE REVISION

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SHEET NO.

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UNDER FLOOR PLAN - PLUMBING

25-43B THE PROPOSED BUILDING FOR LONDELL BUS GARAGE

**GENERAL NOTES:**

- A. THESE PLANS ARE DIAGNOSTIC IN NATURE AND REFLECT THE INFORMATION AVAILABLE AT THE TIME OF THE DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING EXISTING CONDITIONS AND FIELD SURVEY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING EXISTING CONDITIONS AND FIELD SURVEY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING EXISTING CONDITIONS AND FIELD SURVEY.
- B. UNLESS NOTED, EXISTING SHOW IN PLACE. ANY NEW PEN. CALLED ON SCHEDULE SHALL BE SHOWN IN SHOWN NEW.
- C. ALL WORK THAT IMPACTS BUILDING OPERATIONS TO BE SCHEDULED WITH OWNER.
- D. ALL PENETRATION THROUGH A FIRE RATED ASSEMBLY MUST BE SEALED WITH AN APPROVED RATED SEALANT.
- E. WASTE PIPING SHOWN ON RESPECTIVE FLOOR PLANS OCCURS ABOVE CEILING UNLESS OTHERWISE NOTED.

**KEYED NOTES:**

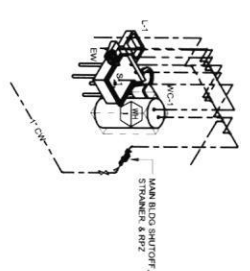
- 1. 1" CW MAIN BUILDING SHUTOFF VALVE. MAT'S 1500 REDUCED PRESSURE BACKFLOW PREVENTER, & STRAINER.
- 2. INSTALL ACCUM CONTROLS WITH THERMOSTATIC MIXING VALVE WITH THERMOMETER SET DISCHARGE TEMPERATURE TO 120°.

**PLUMBING FIXTURE CONNECTION SCHEDULE**

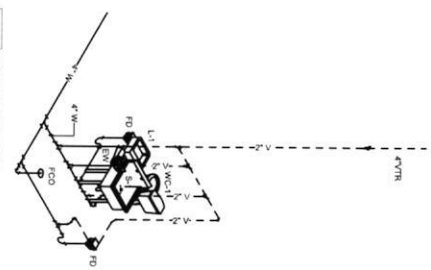
FIXTURE	WATER		WASTE	VENT
	COLD	HOT		
LAVATORY	1/2"	1/2"	2"	2"
WASHER CLOSET	1/2"	-	2"	2"
URINAL	3/4"	-	2"	2"
WASH SINK	1/2"	1/2"	2"	2"
FLOOR DRAIN	-	-	2"	2"
EMC	1/2"	-	2"	2"

\* WASTE PIPING BELOW SLAB TO BE A MINIMUM OF 2".

4 COLD & HOT WATER ISOMETRIC



5 DWV ISOMETRIC



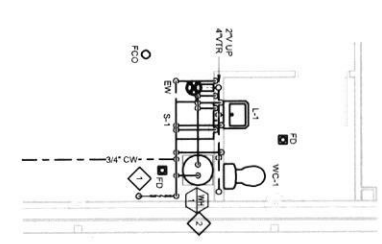
1 FLOOR PLAN - PLUMBING

1/8" = 1'-0"



2 ENLARGED PLAN - PLUMBING

1/4" = 1'-0"



PRELIMINARY  
NOT FOR  
CONSTRUCTION  
95% PROGRESS SET

OWNER: LONEDELL R-XIV SCHOOL DISTRICT  
PROJECT: LONEDELL BUS GARAGE  
NO. E2011924281

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ECC CONSULTING ENGINEERS  
1900 LINCOLN HIGHWAY  
FAIRVIEW HEIGHTS, IL 62208  
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Structural Engineer  
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COCHRAN CIVIL ENGINEERING  
2304 S. KENNEDY DR.  
INDIA, MO 63048  
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THE PROPOSED BUILDING FOR:  
**LONEDELL BUS GARAGE**  
LONEDELL, MISSOURI

**HOENER ARCHITECTS, LLC**  
6707 PLAINVIEW AVENUE  
ST. LOUIS, MISSOURI 63109  
OFFICE: 314.781.9855 HoenerArchitects.com

ALL DIMENSIONS MUST BE VERIFIED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING EXISTING CONDITIONS AND FIELD SURVEY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING EXISTING CONDITIONS AND FIELD SURVEY.

PROJ. NO. 25-438  
DATE ISSUED 05.28.2026  
DATE REVISION

DRAWN BY CAS  
CHECKED BY MW  
SHEET NO. P2.02

PROJ. NO. 25-43B  
 DATE ISSUED 05.28.2026  
 DATE REVISION

DRAWN BY CAS  
 CHECKED BY MW  
 SHEET NO. P3.01

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 ST LOUIS, MISSOURI 63109  
 OFFICE: 314.781.9855

LONDELL R-VY SCHOOL DISTRICT  
 THE PROPOSED BUILDING FOR  
 LONDELL BUS GARAGE

CONSULTING ENGINEERS  
 LONDELL, MISSOURI

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 LONDELL, MISSOURI

Piping Material Schedule

SYSTEM	PIPING			FITTINGS			OPERATING			FIELD TEST			INSULATION			
	SIZE	MATERIAL	SCH	ASTM	JOINTS	MAT.	SCH	JOINTS	PRESS (PSI)	TEMP (F)	PRESS (PSI)	TYPE	TIME	THICKNESS	JACKET TYPE	
DOMESTIC HOT/COLD WATER	4-2"	CP	L	B 88	SJ	CP	L	SJ	65	45-140	150	PNEU	1 HR	1"	ASJ	FG
DOMESTIC HOT/COLD WATER	3/2"	CP	L	B 88	SJ	CP	L	SJ	65	45-140	150	PNEU	1 HR	1"	ASJ	FG
DOMESTIC UNDERGROUND	ALL	CP	K	B 88	SJ	CP	K	SJ	65	45-65	150	PNEU	1 HR	-	-	-
STORM & SANITARY ABOVE GROUND (BELOW CEILING)	ALL	PVC	DWV	D 2685	SW	PVC	DWV	SW	N/A	N/A	10 FT	HYDRO	1 HR	-	-	-
STORM & SANITARY ABOVE GROUND (BELOW CEILING)	ALL	CI	DWV	A 88	NH	CI	DWV	NH	N/A	N/A	10 FT	HYDRO	1 HR	1/2"	ASJ	FG
VENT (BELOW CEILING)	ALL	PVC	DWV	D 2685	SW	PVC	DWV	SW	N/A	N/A	10 FT	HYDRO	1 HR	-	-	-
VENT (ABOVE CEILING)	ALL	CI	DWV	A 88	NH	CI	DWV	NH	N/A	N/A	10 FT	HYDRO	1 HR	-	-	-

**JOINING METHOD LEGEND**  
 NJ - MECHANICAL JOINT  
 NH - NO-HUB  
 S - BRAZED JOINT - SILVER BRAZING ALLOY  
 SJ - SOLDER JOINT 95-5 TIN-ANTIMONY  
 SW - SOLVENT WELD  
 THRD - THREADED

**INSULATION TYPE LEGEND**  
 FG - FIBERGLASS  
 ASJ - ALL SERVICE JACKET  
 \* INSULATE HORIZONTAL STORM ONLY

**FITTING MATERIAL LEGEND**  
 BLK - BLACK STEEL PIPE  
 CI - CAST IRON  
 CP - COPPER  
 ERW - ELECTRIC RESISTANCE WELD BLACK STEEL  
 PVC - POLYVINYL CHLORIDE

**JOINING METHOD LEGEND**  
 NJ - MECHANICAL JOINT  
 NH - NO-HUB  
 S - BRAZED JOINT - SILVER BRAZING ALLOY  
 SJ - SOLDER JOINT 95-5 TIN-ANTIMONY  
 SW - SOLVENT WELD  
 THRD - THREADED

**PIPE MATERIAL LEGEND**  
 BLK - BLACK STEEL PIPE  
 CI - CAST IRON  
 CP - COPPER  
 ERW - ELECTRIC RESISTANCE WELD BLACK STEEL  
 PVC - POLYVINYL CHLORIDE

**PLUMBING FIXTURE SCHEDULE**

Type Mark	Description
EW	ACORN 5048-BF BARRIER FREE WALL MOUNTED EYEFACE WASH WITH STAINLESS STEEL BOWL & WRAP AROUND SKIRT, ABS PLASTIC EYEFACE, WASH SPRAY HEADS SUPPLIED BY INTEGRAL 3.7 GPM FLOW CONTROL, INLINE STRAINER AND STAY OPEN SUPPLIES & STOPS. ASSE 1071 THERMOSTATIC MIXING VALVE FOR EMERGENCY TEMPERING TO 98°F-110°F.
FCO	ZURN EZC-6 FLOOR CLEANOUT WITH CONCRETE SHIELD, ADJUSTABLE COLLAR AND 6" ROUND POLISHED NICKEL-BRONZE COVER.
FD	ZURN S8124 FLOOR MOUNTED 27"X27"X14" 304 STAINLESS STEEL SINGLE COMPARTMENT SCULLERY SINK, CHICAGO BOWL, BEIMS 1055SSC OPEN FRONT SEAT W/O COVER, COLOR: WHITE
FPHS	WOODPECKER NON-FREEZE WALL HYDRANT, SELF-DRAINING, ANTI-SPIROR, 3/4" MALE, 1" FEMALE THREADED PIPE CONNECTION, ALL BRONZE INTERIOR PARTS, INTEGRAL BACKFLOW PREVENTER, 3/4" HOSE THREAD CONNECTION.
L-1	SLOAN SS-3003 ACCESSIBLE 20" X 18" 4" CENTER SET WALL-HUNG WIDESPREAD LAVATORY, CHICAGO 802-VZ805 0.5 GPM PRESSURE COMPENSATING 4" CENTER DECK MOUNT CHROME PLATED CURVED CAST BRASS SHOWER ARM, 1/2" MTR, BRASS PRE-CUT LAV SHIELD, MOUNT TOP OF RIM @ 34" A.F.F., MOUNT CONTROLS 5710 MIXING VALVE SET TO 110°F AS HIGH AS CLOSE TO BOTTOM OF FIXTURE AS POSSIBLE.
S-1	ELKAY S8124 FLOOR MOUNTED 27"X27"X14" 304 STAINLESS STEEL SINGLE COMPARTMENT SCULLERY SINK, CHICAGO BOWL, BEIMS 1055SSC OPEN FRONT SEAT W/O COVER, COLOR: WHITE
WC-1	SLOAN WETS-6023-8013 ACCESSIBLE 1.6 GAL FLOOR MOUNT BOTTOM OUTLET PRESSURE ASSIST WATER CLOSET, ELONGATED

**Water Heater Schedule**

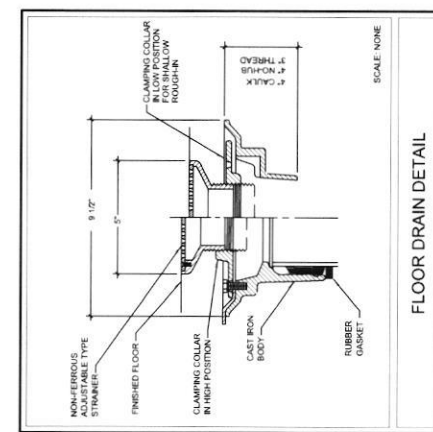
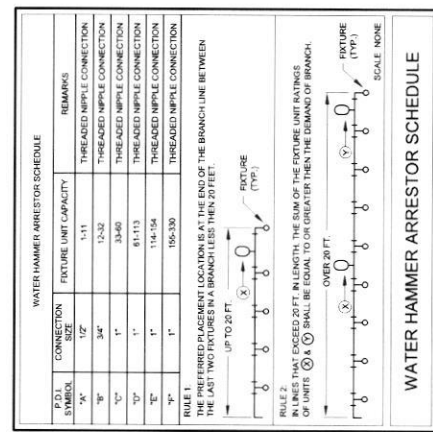
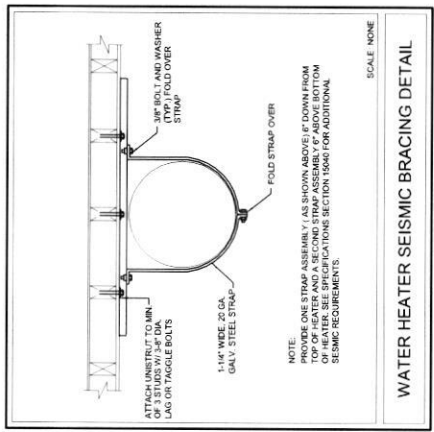
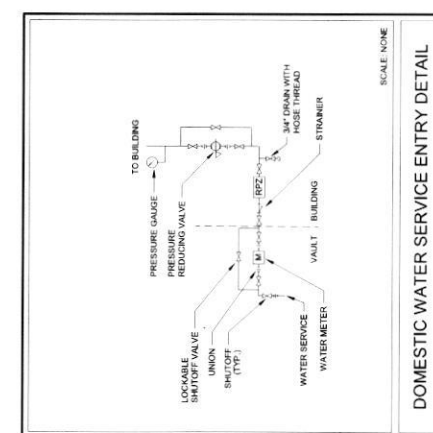
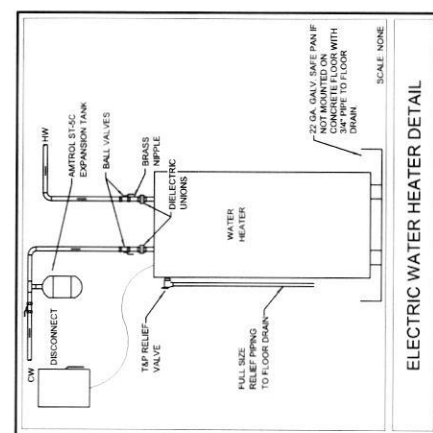
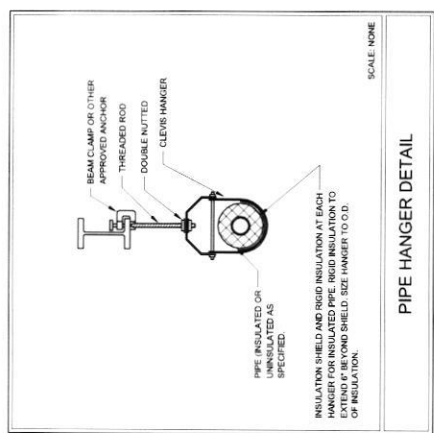
TAG	NUMBER	Manufacturer	Model	WT GAL	HSE	INP (KW)
WH1	1	Bradford White	LEZ2653-J	49	21	5

NOTES:  
 INCLUDE PRESSURE TEMPERATURE RELIEF VALVE  
 UNIT TO BE 208V...

**COLD WATER BUILDING SUPPLY CALCULATION**

FIXTURE	MARK	AMOUNT	N.S.F.U.	TOTAL
LAVATORY (PUB)	L	1	2	2
WATER CLOSET (PUB)	WC	1	5	5
EYE WASH	EW	1	1.4	1.4
SINK	S	1	3	3
WALL HYDRANT	HB	1	4	4
				1 @ 7.5 FPS 2 FPS (SLOPED)

\* WATER CLOSET BASED ON PUBLIC 1.6 GPM FLUSH VALVE  
 \*\* BASED ON THE I.P.C. - INTERNATIONAL PUBLIC CODE





PRELIMINARY  
NOT FOR  
CONSTRUCTION  
85% DESIGN  
PROGRESS

CASTER CHAMBER  
LICENSE #2017018039  
ENGINEERING CORP  
NO. E-20118642187

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CONTRACT ENGINEER  
T. KIM (MSH)  
REGISTERED PROFESSIONAL ENGINEER  
NO. 000000000  
1300 KENTON HIGHWAY  
ST. LOUIS, MO 63104  
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PROJ. NO. 25-43B  
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SHEET NO.

E3.01

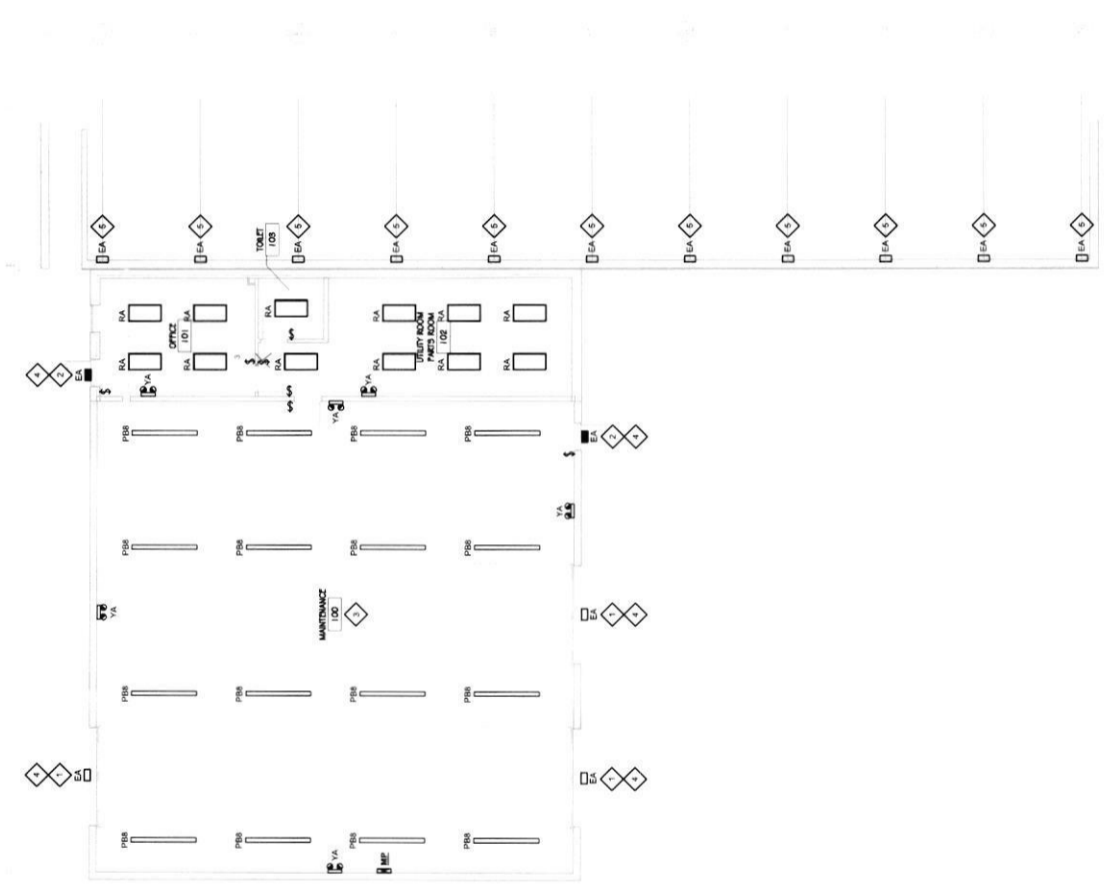
25-43B THE PROPOSED BUILDING FOR LONDELL BUS GARAGE

**GENERAL NOTES:**

- A. DO NOT SCALE DRAWINGS. REFER TO ARCHITECTURAL PLANS AND ELEVATIONS FOR LOCATION AND ORIENTATION OF DEVICES. WHERE LOCATIONS ARE NOT SHOWN, DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE (NEC) AND THE NATIONAL FIRE ALARMS CODE (NFPA 70).
- B. ALL DEVICES SHOWN ARE NEW UNLESS NOTED OTHERWISE.
- C. ALL DEVICES SHALL BE INSTALLED IN WALLS AND CEILING OR BELOW FLOORS UNLESS NOTED OTHERWISE.
- D. WHERE EXPOSED CONDUIT IS NOTED, CONTRACTOR SHALL VERIFY ROUTING IN FIELD AND OBTAIN ARCHITECT'S APPROVAL OF ROUTING PRIOR TO ROUGH-IN. EXPOSED CONDUIT SHALL BE ROUTED TIGHT TO STRUCTURE.
- E. BRANCH CIRCUITS AND HOME RUNS SHALL BE ROUTED IN ENT CONDUIT WITH UNLESS NOTED OTHERWISE OR REQUIRED OTHERWISE BY CODE OR APPLICATION.
- F. MC TYPE CABLE MAY BE USED ONLY WHERE CONCEALED FROM VIEW IN WALLS OR CEILING FOR CONNECTION BETWEEN DEVICES. HOME RUNS AND PANELBOARD FEEDERS SHALL BE IN CONDUIT.
- G. MINIMUM WIRE SIZE SHALL BE #12. MINIMUM CONDUIT SIZE SHALL BE 1/2". UNLESS NOTED OTHERWISE.
- H. ALL SINGLE PHASE BRANCH CIRCUITS SHOWN WITH COMMON NEUTRAL SHALL BE INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE (NEC) ARTICLES TO MEET NEC SECTION 210.4 MULTIWIRE BRANCH CIRCUIT REQUIREMENTS. REFER TO DRAWINGS FOR CIRCUITS WITH DEDICATED NEUTRALS.
- I. SHARED FIXTURES AND FIXTURES NOTED WITH "AFC" ARE FOR EMERGENCY USE ONLY. THESE FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE (NEC) ARTICLE 780. EMERGENCY BALLAST AND INTEGRAL TEST SWITCH FOR EACH FIXTURE REFER TO PLANS FOR QUANTITY AND TYPE OF FIXTURES EFFECTED. WHERE SHOWN, THE SWITCHES PROVIDED SHALL BE INSTALLED IN THE SAME CIRCUIT SERVING LIGHTS.

**KEYED NOTES:**

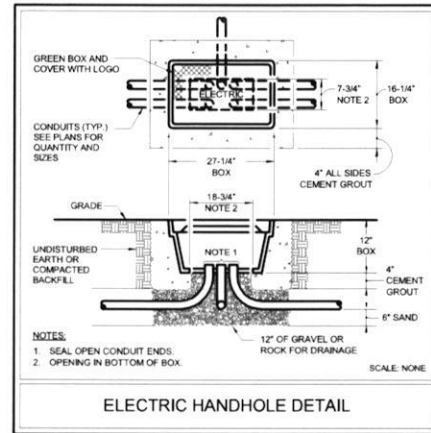
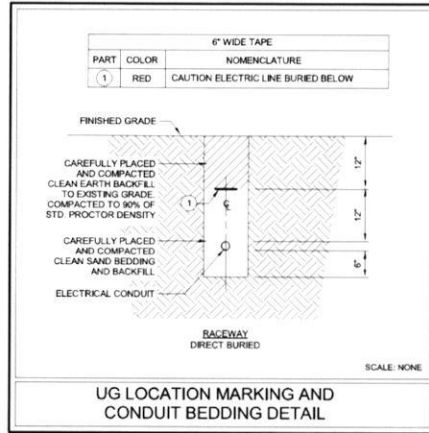
- 1. CIRCUIT EXTERIOR LIGHT THIS LOCATION TO PANEL MP VIA TIMECLOCK TC-1PHOTOCELL
- 2. CIRCUIT EXTERIOR LIGHT THIS LOCATION TO PANEL MP VIA TIMECLOCK TC-1PHOTOCELL AND EMERGENCY LIGHTING INVERTER E/LA.
- 3. INSTALL LIGHT FIXTURES THIS AREA AT 15' AFF.
- 4. INSTALL LIGHT FIXTURES THIS LOCATION AT 15' AFF. COORDINATE FINAL WITH ARCHITECT PRIOR TO COMMENCING WORK.
- 5. INSTALL LIGHT FIXTURE THIS LOCATION AT 15' AFF OF BUS PARKING LEVEL. COORDINATE FINAL WITH ARCHITECT PRIOR TO COMMENCING WORK.



18" = 1'-0"  
1 REFLECTED CEILING PLAN  
E3.01



REFLECTED CEILING PLAN - ELECTRICAL



**Panel: MP**  
 Location: MAINTENANCE 100  
 Supply From:  
 Mounting: Surface  
 Enclosure: Type 1

Volts: 120/208 Wye  
 Phases: 3  
 Wires: 4

A.I.C. Rating: 35,000  
 Main Type: MCB  
 Main Rating: 400 A  
 MCB Rating: 400 A

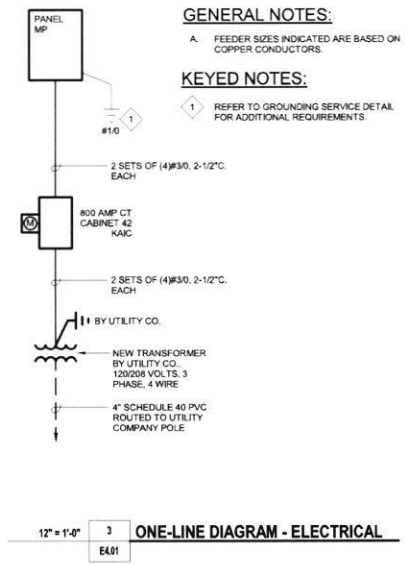
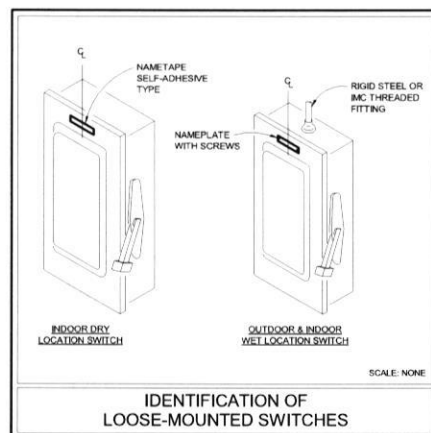
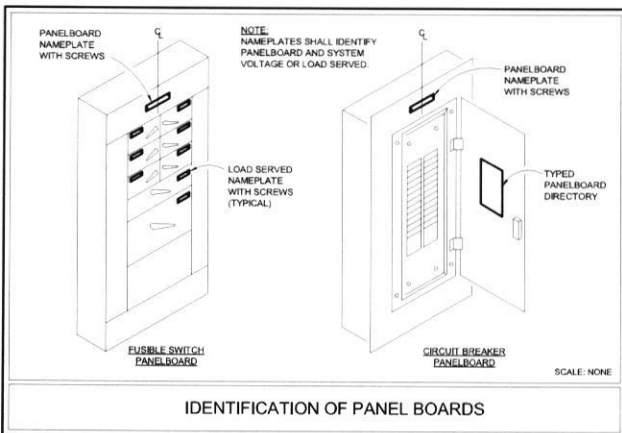
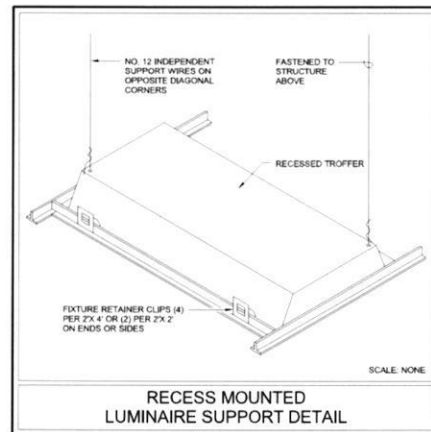
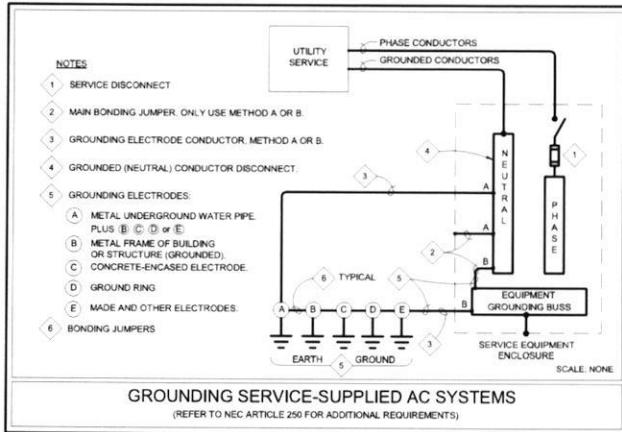
Notes:

CKT	Circuit Description	Trip	Poles	A	B	C	Poles	Trip	Circuit Description	CKT
1										2
3										4
5										6
7										8
9										10
11										12
13										14
15										16
17										18
19										20
21										22
23										24
25										26
27										28
29										30
31										32
33										34
35										36
37										38
39										40
41										42

Total Load: 0 VA    0 VA    0 VA  
 Total Amps: 0 A    0 A    0 A

Load Classification	Connected Load	Demand Factor	Estimated Demand	Panel Totals
				Total Conn. Load: 0 VA
				Total Est. Demand: 0 VA
				Total Conn. A: 0 A
				Total Est. Demand: 0 A

Notes:



"PRELIMINARY" NOT FOR CONSTRUCTION 85% PROGRESS SET

CARTER SCHAMEL  
 LICENSE #2017019039  
 ENGINEERING CORP.  
 NO. E-20118042187

LONEDELL, MISSOURI  
 CONSULTING ENGINEERS  
 M.P.F. ENGINEER  
 FOR CONSULTING ENGINEERS  
 608 COMBUSTION ENGINEERS  
 ST. LOUIS, MISSOURI 63109  
 1. TELEPHONE

LONEDELL R-04V SCHOOL DISTRICT  
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DRAWN BY JB  
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E4.02

DRAWN BY: JB  
 CHECKED BY: CS  
 SHEET NO.

DATE ISSUED: 05.28.2026

PROJ. NO. 25-43B

DATE REVISION

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 HOENERA@architects.com

LOHDELL RAY SCHOOL DISTRICT  
 THE PROPOSED BUILDING FOR  
 LOHDELL BUS GARAGE

CONSULTING ENGINEERS  
 LONDELL, MISSOURI  
 ME: Engineer  
 SE: Engineer  
 License # 2017019039  
 License # 2017019039  
 License # 2017019039

PREPARED FOR  
 CONSULTING ENGINEERS  
 LONDELL, MISSOURI  
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